Downtown Moncton
Development Vision

CITY OF / VILLE DE
MONCTON

DOWNTOWN
MONCTON
CENTRE-VILLE

September **2006**

Downtown Moncton Development Vision

i.   What is this document? How to use it?

The Downtown Moncton Development Vision is a document commissioned in 2006 by the City of Moncton and Downtown Moncton Centre-Ville Inc. (DMCI).

The Downtown Moncton Development Vision is a high level vision document intended to guide development and investments in Downtown. The Development Vision is most effective when it can compel, inspire and bring about the intended outcomes. To do so, it is rooted in the realm of possibilities, is prepared for a broad spectrum of users, and provides clear and concise direction for decision-makers. To be plausible, the Development Vision has been substantiated through analysis, is a product of a well documented public consultation process, and succinctly defines the appropriate tools and or mechanisms for implementation.

The Development Vision for Downtown Moncton has been divided into three documents to make it manageable and easily referenced. These are:

The Background Report

The Background Report provides the documentation of the analysis and process that is the basis of the Development Vision. This document provides a summary of the outcomes of the research, analysis and consultation that supports the vision contained in the Development Vision and the potential strategies that bring it to fruition.

The Development Vision

← this document

This is the long term physical plan and concept for Downtown. It presents the intended physical structure that will guide decisions regarding the character of uses, open spaces, and the built form over the next 25 years. Of the three documents, the Development Vision will have the most longevity, given that it describes a vision for Downtown.

This will be useful for residents, land owners and developers as well as the public sector, to guide investments, direct planning policies and public improvements, and to function as an economic development tool. It may also act as a reference to assess development applications.

The Implementation Strategy

This is the implementation plan and recommended actions. It sets out potential projects, programs, policies and strategies to move the plan towards its realization. With an emphasis on initial capital improvement projects, this document addresses the current conditions of Downtown by defining the necessary actions that will advance the long-term vision presented in the Development Vision.

The Implementation Strategy document will be useful in guiding the City and Downtown Moncton Centre-Ville Inc. (DMCI) in defining programs, setting priorities, allocating finances and assessing achievements. Over time, this section of the Development Vision should be revisited and updated to ensure that the strategies remain relevant and current to Downtown as it evolves.

ii.   Acknowledgements

This study was managed by the City of Moncton with the participation of Downtown Moncton Centre-Ville Inc (DMCI). Many champions - from the Mayor, to Council, to City Staff and the members of DMCI - were intimately involved with and supportive of the study. Their passion for Downtown was instrumental in leading us through the workshops and debates.

Throughout the Study, community representatives provided invaluable input and comments. These contributions were vital to ensuring we captured the future Vision for Downtown Moncton. Many became further involved through our Advisory Committee. Members of the Greater Moncton Planning Commission, as well, provided an invaluable contribution to the process.

Special thanks to the Capitol Theatre for lending space and other resources during the Symposium. Hosting this event in a landmark building located directly in the Study Area was imperative, and appreciated. Undoubtedly it was a better process for this generous contribution.

This event was unique in as much as it sought to integrate a distinct understanding of Downtown with the passionate vision of community representatives who have been engaged in thinking about the area for many years. Without fail, our meetings were dynamic events, full of passion for the City of Moncton. We are certain this Vision is stronger as a result of the many thoughtful contributions we recieved.

ADVISORY COMMITTEE MEMBERS
- Downtown Moncton Centre-ville Inc.
- AshVerd Properties
- YMCA
- Enterprise Greater Moncton
- Moncton Museum
- Centre Cultural Aberdeen
- Université de Moncton
- SOS Eau Water Sankwan Inc
- Codiac Regional RCMP
- Capitol Theatre
- Permanent Settlers Association
- Property Management & Loans Administration
- Construction Acadienne Itée
- Father J. Angus MacDonald Centre
- Northrop Frye Festival
- Assomption Vie
- The Frameworks
- Fortis Properties
- Times & Transcript
- Deputy Mayor, City of Moncton
- Crombie Properties

TECHNICAL CLIENT COMMITTEE MEMBERS
- City of Moncton, Corporate Planning and Policy Development
- Downtown Moncton Centre Ville Inc, DMCI
- City of Moncton, Office of the City Manager
- City of Moncton, Environmental Planning and Management
- City of Moncton, Transportation
- Greater Moncton District Planning Commission

CONSULTANT TEAM MEMBERS
- Office for Urbanism
- Architecture 2000 Inc.
- Infoplexxus Inc.
- Frederick Hann Associates Ltd.

Photographs taken during the Downtown Moncton Development Vision Symposium, June 2006

iii. How to become involved

The implementation of this Vision is the responsibility of all.

While the municipality and the Greater Moncton District Planning Commission will be the stewards of the Vision, implementation of the Vision belongs to residents, community groups, developers, property owners, businesses, and all people who are interested in the success of Downtown.

This Vision will unfold in many ways, through initiatives large and small. As such, it demands the attention of many players.

This, therefore, is a call to action.

As a resident, participate in subsequent consultations. Engage in the conversations about Downtown, use Downtown, walk around and make it part of your everyday life.

As a property owner, be aware that individual buildings and individual lots add up to create the overall experience and identity of Downtown.

For further information and to find how you can become involved, or to obtain a copy of the Development Vision and/or Background Report, visit:

http://www.moncton.ca/search/english/cityhall/downtown/downtown.htm

or contact

Corporate Planning and Policy Development
Planification corporative et élaboration des politiques
City of / Ville de MONCTON
506-853-3459

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INTRODUCTION

1.1 Downtown Development Vision – The Opportunity

The Development Vision emerged from an extensive consultation process centred on a 3-day Symposium at the Capitol Theatre. It is therefore the result of a diverse array of stakeholders engaged in a focused conversation.

The following pages expand on what constitutes the Vision and set forth guidelines to orient residents, developers, City staff, and all other stakeholders such that the Vision is tangible.

Implementing the Vision is also your responsibility.
As a resident, a property owner, a visitor, or a user of Downtown, you will be making choices everyday that have a ripple effect on the quality and viability of Downtown. Assume the responsibility.

Revitalization should be citizen-driven - but should also involve the City, private and non-profit sectors, the University, other educational institutions, and neighbouring municipalities.

Your first project will be the hardest – you will have to resist doing what is "easy".
First projects are by far the most difficult. At times it will be challenging to maintain the vision, and there will be conflicting opinions with respect to what constitutes respect for the Vision.

But,… one high quality, high density project can turn Downtown around.
You can significantly change Downtown in a relatively short period of time. An inspiring building, a stunning open space, will have ripple effects among its future neighbours. Quality begets quality – residents and developers will join in, once the ball gets rolling. The first project could become a symbol, which is why it is so imperative to get it right.

Density also matters. Increasing the number of pedestrians on the street, in shops, and making use of various amenities will increase the viability and appeal other Downtown projects.

Leverage what exists – heritage, culture, landscapes – and create value.
Existing heritage stock is irreplaceable – its defining qualities already create an authentic sense of place. The riverbeds are there – it is only a matter of using them well.

Private investment is needed.
Recruit developers who buy-into the vision. It is imperative to create the conditions necessary to attract development that is in keeping with this Vision. Some of the largest impacts Downtown will result from the private investment of landowners, big and small.

Success will depend on a variety of initiatives unfolding simultaneously.
There is no single course of action, and no silver bullet. The Vision must be pursued through policy, through buildings, through the design of open spaces, through programming, through awareness campaigns, through economic strategies… through the participation of all.

Collaboration is required across sectors.
It is imperative to have a comprehensive approach to implementation that integrates a diversity of perspectives and needs: social services, engineering, detailed design, the needs of both children and the elderly. For this reason, it is imperative to continue to check in with many of the stakeholders who participated during the Visioning process.

Achieving success is a long-term commitment & process.
All successful downtowns are evolving, continuously refining their visions, focusing strategies, and working hard. Downtown Moncton will also evolve, requiring periodic redirection of the implementation strategy to realize the principles of the Vision.

Downtown Moncton must have cheerleaders.
It will require the concerted effort of people (from the City, from its residents) who pick up the Downtown flag and make it happen.

areas of influence
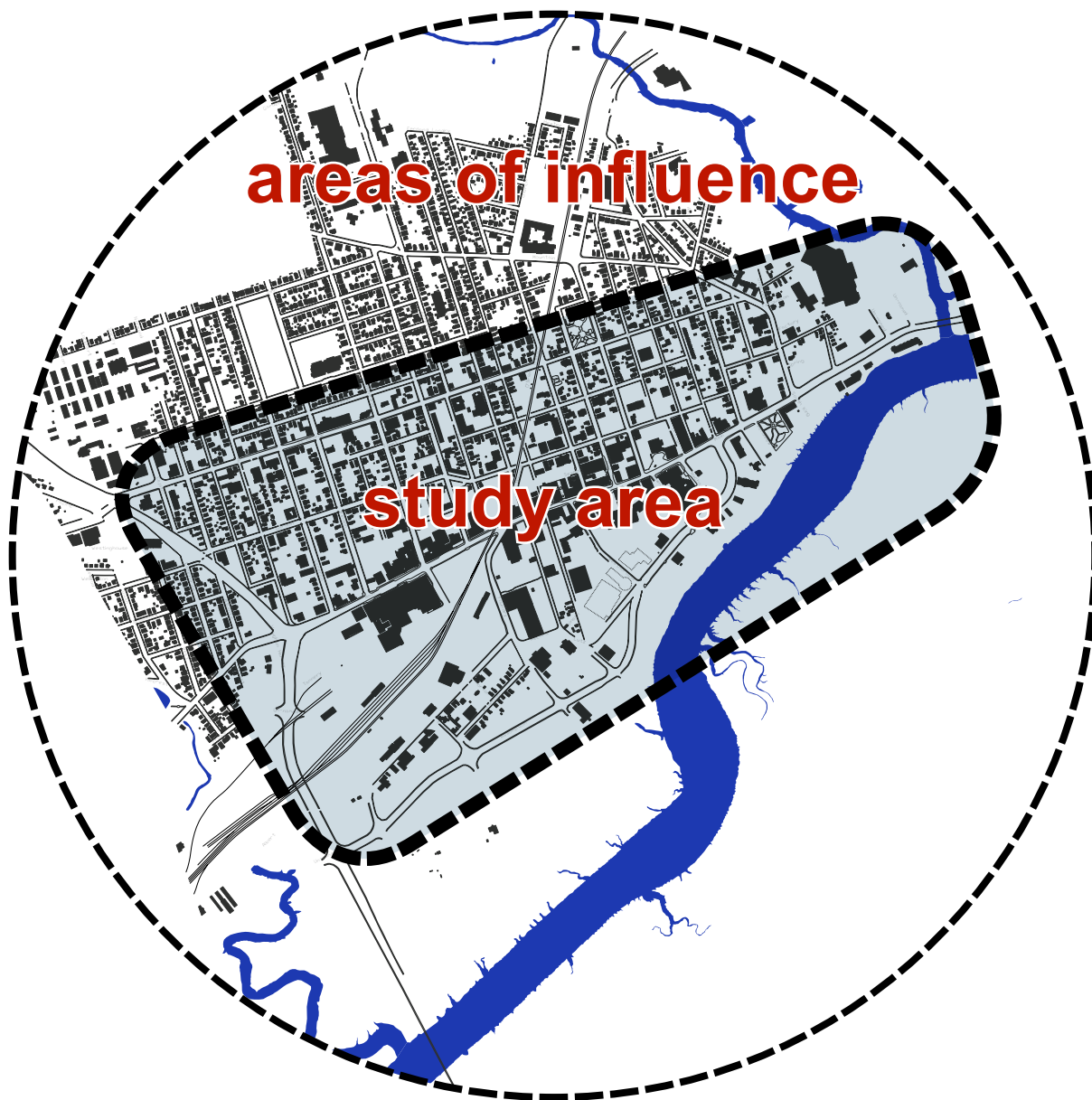
study area

Study Area - For the purposes of this visioning process, the boundaries of Downtown were identified as: the Petitcodiac River (to the south), Vaughan Harvey Boulevard, (to the west), Saint George Street (to the north) and Hall's Creek (to the east). However, the analysis and the consultation extended beyond these boundaries to include the surrounding neighbourhoods and natural features as significant areas of influence.

1.2 Downtown Development Vision – An Overview

A healthy downtown is essential to a City's well-being. Downtowns that are well-designed are well positioned to attract and manage new growth while simultaneously being able to venerate historic urban features such as distinctive block patterns, roads, landmarks, and buildings.

Across North America, Downtown redevelopment has taken a new direction. Small cities are challenging large urban centres for new development and economic growth and their success in doing so is linked with their development visions. Small cities are now leveraging the novelty of their distinctive main street experience to define themselves. The benefits of this type of approach are many. Advantages include generating increased outside interest from tourists and businesses alike. Additionally, a renewed emphasis on Downtown provides a method for managing growth by focusing development into a defined urban area.

As part of a new Downtown Moncton Development Vision, the City has the opportunity to begin the process of urban repair by addressing orphaned spaces throughout the core, but also to seek a new competitive edge, welcoming new industries, constructing distinctive housing opportunities and creating a cultural destination.

Moncton is a unique city that is both a geographical and cultural hub of the Maritimes. Some of Moncton's features that can be captured and enhanced include:
- An attractive and functional main street with a large inventory of historic buildings;
- A vibrant creative community and 'Arts Scene';
- Extraordinary natural features;
- Tremendous opportunity for urban infill;
- A prosperous region;
- Local leadership.

While Downtown Moncton has the potential for positive change, that potential has yet to be realized. Recent commercial investment in the city has displayed non-urban characteristics, detracting from the quality of the downtown experience. Large tracts of urban land are undeveloped or in use as surface parking, contributing to a 'nondescript' feeling in parts of the core. Even though the historic buildings along the central section of Main Street remain largely in tact, there are areas of Downtown that have, over time, lost their unique character. Nevertheless, Moncton's strong identity and growing economy give the City the strength to implement a new vision for downtown development.

Why a create a Development Vision?

The Development Vision establishes a long-term vision and physical plan for an area so as to guide decision-making to be in keeping with that vision. Fundamental questions it answers:
- What do we envision Moncton to be like in 20 years?
- What are the aspects that we treasure and want to protect?
- What are the aspects we feel need to be improved upon?
- What are the aspects we do not like and want to change or discourage?
- How do we make this Vision happen?

Why a create a Development Vision now?

As with many similarly scaled cities, the historic function of the downtown as the primary retail and employment centre has played a critical role in the success of the entire region.

Already, several large developments are anticipated Downtown. A comprehensive vision for Downtown will provide clarity and guide these future initiatives. Providing clarity establishes development parameters and certainty for land owners, businesses, developers and residents. Treasured aspects of the area need to be identified so that they may be protected. Identifying areas which are appropriate for revitalization or improvement will enhance those areas. The Development Vision provides an opportunity to unite the community by

working towards consensus and providing one voice. It also provides a direction for action by identifying priorities for investment. Finally, the Development Vision will assist with the economic development of Moncton by providing tools and materials for marketing and generating investment interests.

Why support a healthy Downtown?

Healthy downtowns facilitate vibrant cultural activities and industries, attract tourists, incubate local independent businesses, accommodate a variety of lifestyle choices and make efficient use of municipal services and infrastructure. These attributes are unique to downtowns because they have the necessary density of people, mix of uses, variety of activities, pedestrian charm, built quality and sense of place that enable this vitality.

Promotes Sustainability and an Efficient Use of Municipal Resources - Downtowns are existing urban environments and therefore redeveloping these areas supports the principles of sustainable development and Smart Growth. Downtown infill and redevelopment projects promote a concentration of growth and a wise use of existing community resources and infrastructure.

Facilitates Civic Life - Traditional downtown core areas are the location of a concentration of civic space and public amenities. These public spaces and amenities provide important forums for community interaction and connection through everyday use, events, and celebrations.

Accommodates a Variety of Lifestyle Choices - Downtown Moncton has the potential to provide alternatives in terms of housing types, services, products, and entertainment which is very different from those found in other areas of the City.

Capitalizes on the Existing Cultural Heritage - The Downtown has the greatest concentration of cultural heritage in Moncton. The importance of conserving heritage resources is understood to be a key factor in creating and supporting healthy and viable communities.

Symbolic and Profile Importance - Downtown is should be a reflection of community accomplishments, economic health, pride, ideals, and values. Most importantly, it should be possible to see the image of the entire City and its quality of life based on the health and quality of its downtown.

Independent Business and Incubators - Downtown provides ideal locations and environments for independent businesses, new businesses and entrepreneurs. The built form affords a number of unique spaces in distinctive environments that enable business to create flagship operations.

Economic Development and Return on Investment - A healthy Downtown generates tourism; improves real estate values, and lowers vacancy rates; enhances spending activity; increases tax assessment; reinvests in building stock; and stabilizes existing businesses and attracts new business.

Ann Arbor, MI

Ann Arbor's Downtown renaissance and continued resilience in Michigan's challenging economic and fiscal context is due in no small part to the mature governance and strategic approach of its Downtown Development Authority.

Asheville, NC

Asheville's Revitalization Commission provided leadership, refocused investment and living Downtown. The Commission nurtured a strong citizen-driven process that attracted private sector champions who galvanized investment momentum and built catalytic projects Downtown.

Boulder, CO

Boulder's acclaimed Pearl Street pedestrian mall has successfully evolved through early, active Downtown intervention by City management into an important regional attraction regularly renewed through public-private collaboration and strong local governance.

Burlington, VT

Careful City planning has allowed Burlington's award-winning Downtown Church Street Marketplace to flourish. A rare success, the mall includes national chain stores as anchors, a strong design synergy with a contiguous indoor mall, and a recognized identity as a regional retail and entertainment district.

San Luis Obispo, CA

Serious flooding of San Luis Obispo Creek in 1969 and 1973 catalyzed a community-driven process that led to the renewal of historic Mission Plaza, a two block area of downtown that has become the physical, cultural, and spiritual heart of the city. The area has thrived despite receiving few public incentives or local taxing powers.

Savannah, GA

Backed by a strong historic preservation movement, it took a key infrastructure commitment downtown to attract private sector investment. Savannah's Downtown Development Renewal Authority has led education, financing and public-private partnerships to support Downtown governance, facilitating the expansion of the Savannah College of Art and Design, a multi-disciplinary art school that has had a profound radiating effect on the cultural and commercial resurgence of the City.

Wilmington, NC

Wilmington's Downtown Area Revitalization Effort initiated early key adaptive reuse projects to help sustain the urban core, including the renovation historic shopping centres that bookend the main commercial street, investment in Downtown's Riverfront Park and the reconstruction historic market district and streets.

1.3 Characteristics of Successful Downtowns

Downtowns are regaining momentum as the most important part of a city. There is recognition by residents and governments that a successful downtown presents a positive image that in turn will attract economic investment and improve the quality of life for the people who live there. Downtown is where most important buildings are located, buildings with the most meaning to a community. These buildings, coupled with other culturally significant spaces, typically result in the greatest concentration of cultural heritage within a City. Moncton is no exception. Conserving these cultural and heritage resources is a key factor in creating and supporting a healthy, viable community.

Downtowns also play an important social function for a City. Downtowns typically have the most diversity: it is here that people across a wide demographic spectrum live side by side.

The following section will look at characteristics typically present in successful Downtowns. It is important to note that there is no single model or design that can be applied to revitalize a downtown, but rather some or all of the commonalities should be present, while respecting the unique character and attributes of different communities.

Multiple Traffic Generators Within Short Walking Distance

Many downtowns in small cities have multiple traffic generators. Most people view Downtown as a destination, but in actuality, many downtowns have multiple destinations that are within close proximity to one another. Having multiple traffic generators within close proximity promotes pedestrianism and makes walking a desirable option.

For Downtown Moncton the key will be not only to create different traffic generators, but also to attract a range of demographics, during different times of the day and during different seasons.

Loved Spaces

Successful downtowns typically have the highest concentration of open spaces in the City. As a result of the unique character, people love their downtown and recognize the importance of ensuring its viability and vitality.

Downtown Moncton is privileged to have many loved spaces. The current momentum of creating recreational connections to the Petitcodiac Riverfront and Hall's Creek will only augment them.

Adaptability

The most valuable characteristic of Downtowns is their capacity to adapt in order to overcome challenges and obstacles. Downtowns are typically located within close proximity to a City's original settlement. Their location was often chosen based on its natural features such as access to a waterway, or defensibility. These assets make it desirable to evolve in place.

As Downtown Moncton evolves from a port / industrial base into a service economy, the challenge and the opportunity is to craft the quality of the urban environment that is conducive to the greatest economic development by enhancing the quality of life for local residents, workers and visitors alike.

Pedestrian Scale

Most downtowns have small, walkable blocks. Most downtowns were built for a different era, before the advent of the automobile. They were designed to be easily accessible to pedestrians and as such, are characterized by a fine grain street pattern laid out in a grid to provide people with as many options as possible for getting from A to B.

Downtown Moncton is a perfect example of this pedestrian scale. The challenge will be to extend this character where it has been lost, such as in the railway lands.

Invigorating or developing a "main street" condition, with an attractive pedestrian environment, including active retail at grade and residential above, is often the focus of downtown revitalization plans.

Mixed-Use Development

Downtown is a city's 'centre.' It needs to appeal to a diverse range of interests and meet the needs of a wide range of people. The recognition of the role and function of downtowns is re-emerging across North America, including the recognition that there must be a diverse range of uses to satisfy as diverse a range of needs as possible.

Mixed-use development encourages 24-hour, 7-day-a-week activity on streets. Providing a place where people can live, work and play ensures that Downtown does not get deserted after the offices and stores have closed. Most downtowns have restaurants, theatres and most importantly, a strong residential base. People are able to engage in activities outside of retail and office hours, contributing to a vibrant and active environment.

The balance of uses in Downtown Moncton has shifted overtime. Maintaining a balance will require creative strategies and a focus on the overall livability of the urban environment.

Broad Private and Public Investment

The majority of successful downtown revitalizations in North America were accomplished through private/public partnerships. Although there needs to be investment from both the private and the public sector, private dollars have typically surpassed public investment by a ratio of 15:1. This is primarily due to the fact that the public sector is not equipped to lead real estate development and a lengthy downtown turnaround.

That being said, the public sector typically plays a crucial role by initiating the process and creating a strategy that will guide investments by the private sector.

In this regard it is critical to acknowledge that a successful Downtown for Moncton is the responsibility of everyone. The key responsibility of the public sector will be creating a strong regulatory framework that both inspires and directs investment.

The Nature of Downtown Retailing appears to be in Flux

Successful downtowns across North America have recognized that they cannot and should not be competing with big-box retail and indoor shopping centres. Downtowns are enhancing their unique cultural heritage and pedestrian friendliness as a key competitive factor to differentiate their downtown from other types of retail with great success. Downtowns are capitalizing on the emotional attachment people have with heritage buildings and public open spaces. This does not exist with suburban big-box retail.

In the past Moncton has felt significant pressure to approve any development within Downtown. Times are changing. Now, the creation of a Downtown Vision will allow a focus on the overall contribution of any development to the public realm, and to the long-term Vision.

Entertainment is a Driving Market Segment

Successful downtowns have used entertainment as a driving force for changing the fortunes of their downtowns. Those aged 20-30 as well as older "empty nesters" have been recognized as the key demographic attracted to residing in urban areas. To attract them to live downtown, there needs to be urban entertainment – venues and retail space that are within walking distance to one another. Urban entertainment has included arenas, arts and culture centres, theatres, restaurants, nightclubs, specialty retail, and convention centres.

Downtown Moncton is well advanced on this route. The next few years will be critical in enhancing a wide spectrum of amenities and entertainment venues that are compatible with our emerging residential market.

Critical Mass

What makes Downtowns successful are people. Having a large residential base living in the downtown creates vibrancy and activity on the streets at all times during the day, every day. People on the street create the perception of safety, which in turn will attract more people to the area.

A strong residential base downtown will also attract neighbourhood retail, such as grocery stores, to locate nearby.

Downtown Moncton has ample potential to incorporate a substantial residential base. The Vision will demonstrate how.

2

AREA HISTORY

1899 - Riverfront

1906 - Moncton Fire

n.d. - Tidal Bore

1907 - Hotel American

1923 - Snowstorm

2. Area History

The history of Moncton is rooted in a rich, cultural diversity that is still present today. The area was originally settled by the Mi'kmaq first nations. In 1733, French Acadians began to settle in and around what is presently the Greater Moncton Area. The Acadians established a farming community called "Le Coude" which translates into the Elbow, referring to the settlement's location along the bend in the Petitcodiac River.

In 1755, the Petitcodiac River Valley fell under British control after the capture of Fort Beaséjour, and the Acadians were deported, leaving the settlement relatively vacant until 1766 when a group of eight immigrant families arrived from Pennsylvania. The families were armed with land grants that had been issued by the Philadelphia Land Company, with one of the principal investors being Benjamin Franklin. The settlement that began to grow was named "the Bend of the Codiac."

Moncton was officially incorporated as a town in 1855, during which time Moncton flourished as a centre for ship-building. In 1862, Moncton was forced to surrender its Charter as the birth of steam and iron ships brought a quick end to local prosperity.

It was not until 1875 that Moncton regained its charter and was again incorporated. This time Moncton used the Latin motto 'resurgo' (I rise again) to describe Moncton's ability to adapt and prosper regardless of the obstacles that had been put in its path.

'Resurgo' proved to be a fitting motto for Moncton in 1877, when the Inter-Colonial Railways, the forerunner of Canadian National Railways, decided to locate their headquarters in Moncton. With the headquarters for the Inter-Colonial Railways, Moncton emerged as the 'hub' of the Maritimes; officially recognized as a City on April 23rd, 1890.

On February 24, 1906, Moncton was devastated by one of the most spectacular fires in the City's history. The fire began in the Inter-Colonial Railway shops on Albert Street and quickly spread to other parts of the City. When the fire was finally put out, there was millions of dollars worth of damages. The rebuilding was done quickly to calm fears that the shop facilities would relocate to Halifax or Riviere-du-Loup.

On December 11, 1911 history repeated itself when gas from the Stoney Creek field was turned on and several businesses were destroyed by a gas explosion. The extent of the damages were not of the same scale as those caused by the 1906 fire, but there were still extensive damages with the cause of the explosion being linked to a leak in one of the stores basements.

The T. Eaton Company opened its new Maritimes mail order house in Moncton on February 5, 1920. The operation employed 750 people and was so successful that Eaton's added a retail department to the complex.

The 1960's brought two significant events that would have a significant impact on the City. The Trans-Canada Highway Network was built along the northern edge of the City and the infamous causeway was constructed on the Petitcodiac River. It was also during this time that the Université de Moncton was established on July 15, 1963.

Moncton's history is as much a part of heritage as it is about its future. Like all Cities, key decisions and events have shaped what the City has become today. After a period of economic decline, the City of Moncton is now prospering once again as businesses are capitalizing on the City's unique qualities such as its bilingualism and its central location within Atlantic Canada.

1930 - Main Street

1906 - Main Street

Did You Know?

Moncton was originally named Le Coude with the first European settlements established in 1733.

Half of the businesses in the 700 block on Main Street were destroyed in a natural gas explosion in 1911.

Recently, Canadian Business Magazine voted Moncton "the best place to open a business in Canada."

Greater Moncton boasts the best rate of in-migration east of Ontario, due in part to the low cost of living, which is among the lowest among urban areas in North America.

There used to be a streetcar running down the centre of Main Street that ceased operations in 1931.

In 2006, Moncton was the leading City in Eastern Canada as a tourist destination.

The 'subway' was chosen through a plebiscite as the preferred solution for dealing with how Main Street interacted with the railway tracks.

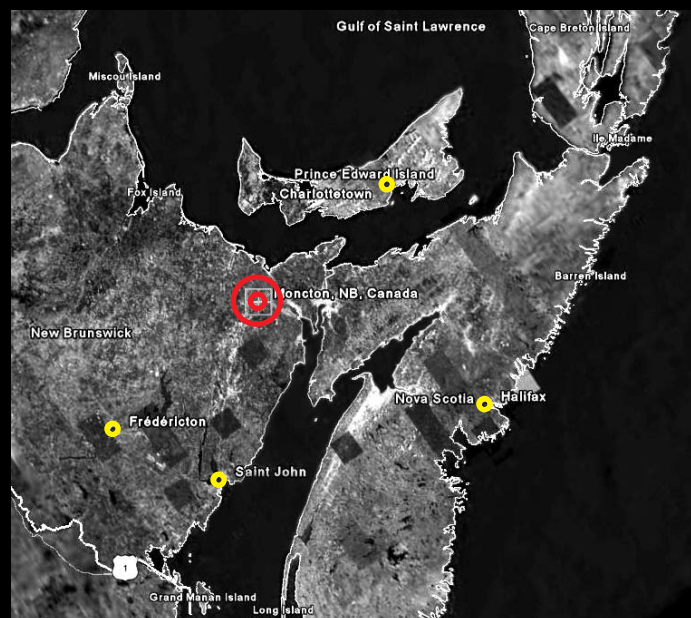3 photos of the 'subway' above are courtesy of the personal archive of Richard Landry.

3

CONTEXT ANALYSIS

3.1.1   Moncton in Canada

- Moncton is located in the southeast part of the Province of New Brunswick in Canada.
- It is the largest city in New Brunswick with a population of 61,046.

3.1.2   Moncton in the Maritimes

- Moncton is at the centre of the Maritimes. It is approximately a two hour drive to Saint John, Halifax, Charlottetown and Fredericton.

3.1 Context & Study Area

Downtown Moncton possesses a number of characteristics that help define it as a unique place within the City. These characteristics, the physical patterns, patterns of circulation, land uses and functions that the downtown supports, are what work to define Downtown. The following pages present a brief summary of the characteristics that have been fundamental to understand in order to generate the Development Vision.
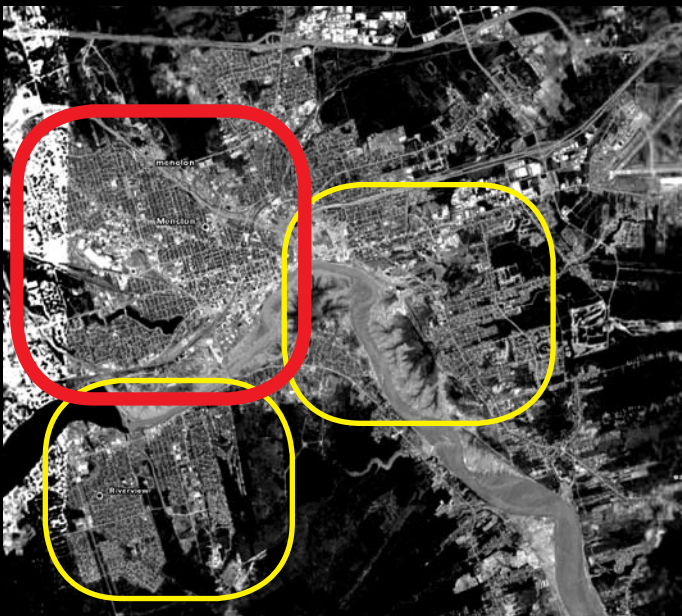
3.1.3   Moncton, Dieppe and Riverview

- Moncton is located west of Dieppe and north of Riverview; it is in the centre of the three communities. Together the three cities make up the Greater Moncton Area.

3.1.4   Downtown within Moncton

- Downtown is located in the southern part of the City along the Petitcodiac River.
- Downtown is where the concentration of services is located and has historically been the centre of activity for the region.

3.2 Urban Structure and Physical Patterns

3.2.1 Movement and Access Points

There are 9 primary vehicular access points to Downtown Moncton:
- Vaughan Harvey Blvd.
- Main Street
- Assomption Blvd.
- St. George St.
- Mountain Rd.
- Botsford Street
- University Avenue

Rail Corridor
Major Arterial
Arterial
Mixed Main Street
Arterial - Scenic Opportunities
Street
Trails
Parking

3.2.2 Street Network

Downtown Moncton has a system of connecting streets, which function to orient and distribute vehicular traffic, including: Vaughan Harvey Blvd., Mountain Road, Botsford Street and St. George Street. west of Vaughan Harvey Blvd. Downtown is also characterized by several mixed-use main streets that have a diverse range of uses and typically have two lanes of traffic with one or two lanes of on-street parking, including: Main Street and St. George Street (east of Vaughan Harvey Blvd.). Streets that edge the river have particular scenic opportunities, including: Assomption Blvd. and the Vaughan Harvey Bridge.

Points D'Acces / Access Points
Déplacement / Movement
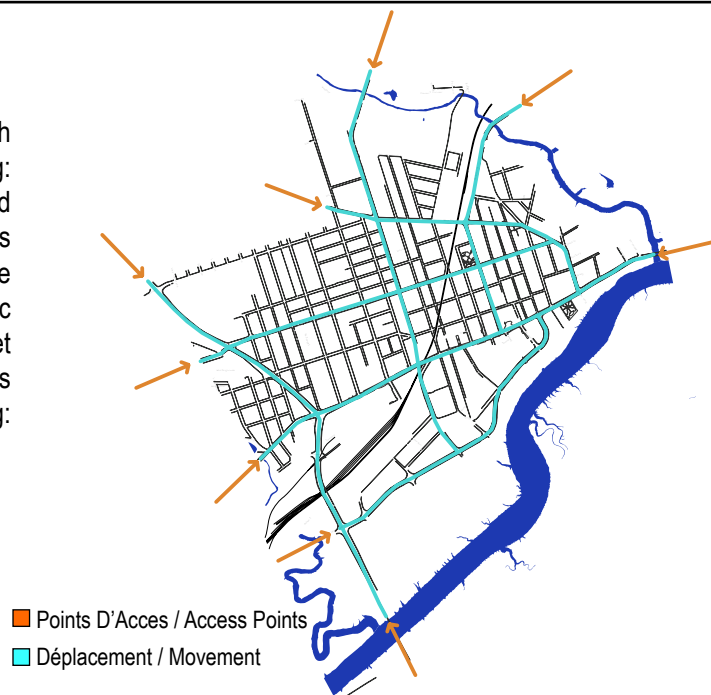
3.2.3   Pedestrian Movement and Trails

Pedestrian routes exist along all streets in Downtown Moncton, with the majority of streets having paved sidewalks. The only existing pedestrian trail exists south of Assomption Blvd along the riverfront.


3.2.4   Public Transit

Downtown Moncton is served by Codiac Transit. Public transit is provided by bus service covering all Downtown main streets, connecting the City to Dieppe and Riverview. There is a well-used concentration of bus stops located on Main Street just north of Highfield Square Mall.
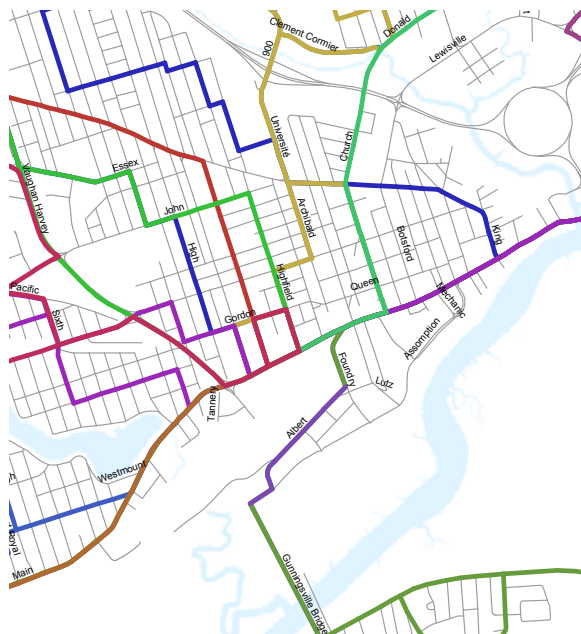

City of Moncton - Bus Routes Map

3.2.5   Urban Development Pattern

Historically, Moncton developed along the river, but over the years, new development has turned away from the river. There is a gradual transition from the industrial uses that were historically located along the riverfront to a variety of different uses, including retail and residential. The opportunity exists to re-define the river edge with a renewed urban development pattern north of Assomption Boulevard.

■ Parks
■ River
■ Built Form
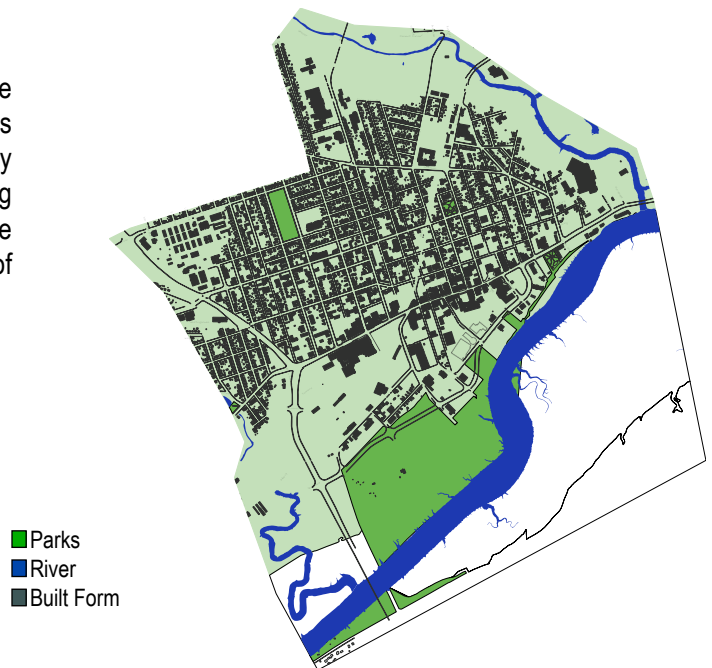
3.2.6   Block Size and Pattern

A fairly consistent grid exists north of Main Street. This grid provides clarity, efficiency and a sense of orientation. South of Main Street (responding to a port / industrial history) the grid is modified, with fewer streets, larger blocks, and limited access.

□ Block Pattern
■ River
■ Parks
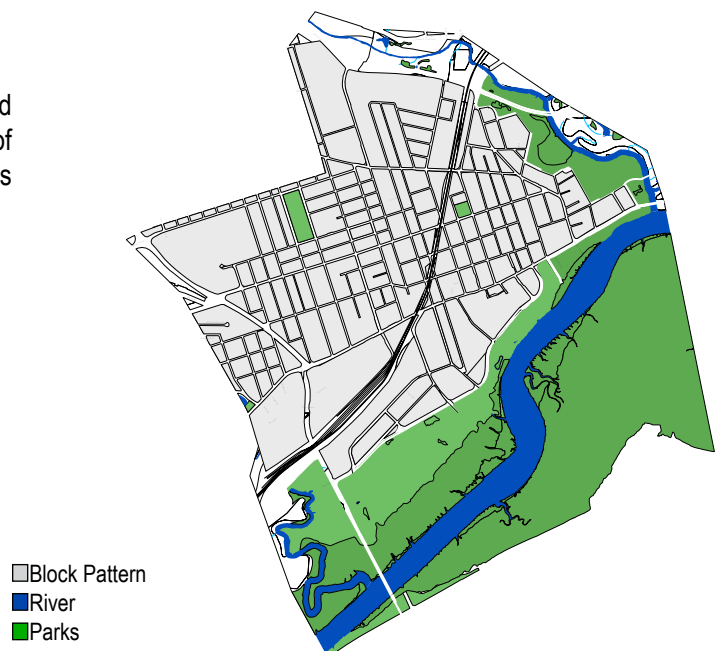
3.2.7 Building Footprint

In general, Downtown is characterized by smaller building footprints that reflect residential and traditional retail uses. Larger building footprints dominate the area south of Main Street, with the exception of the area between Botsford Street and the Subway, which is characterized by larger footprints that appear smaller as a result of façades that 'break-up' the large building frontages.

3.2.8 Underutilized Sites

Five significant clusters of underutilized areas exist within Downtown. These sites present significant opportunities to both redefine Downtown and to inject some of the critical new uses, such as residential, that are necessary to add vibrancy.
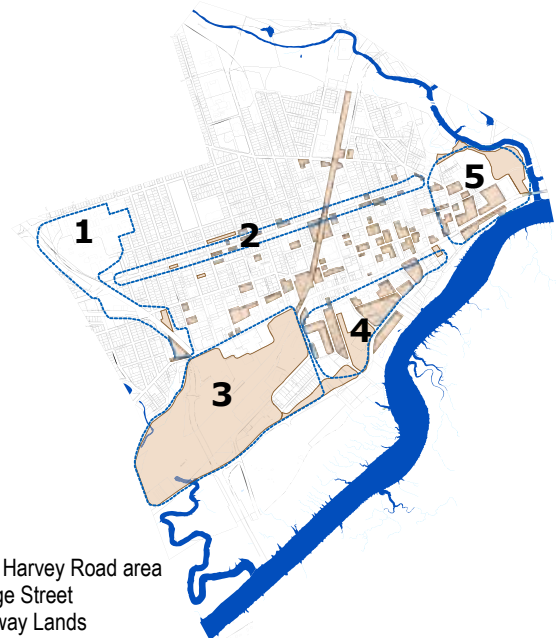
1. Vaughan Harvey Road area
2. St. George Street
3. The Railway Lands
4. The Riverfront Lands
5. Main and King area

3.2.9   View Corridors + View Termini

A view terminus is the end point of a view line. It is often accentuated through design elements such as public art, adding/subtracting from the building mass, or landscaping.
A view corridor is the path or route (often a street, but sometimes an open space) that leads to a view terminus.
The orientation of the street grid provides for many view termini and corridors; some have yet to be captured through the urban design of Downtown.

↖ View Corridors

● View Terminus

3.2.10 Landmark and Heritage Sites

A landmark is a building or structure that stands out from its background by virtue of height, size or some other aspect of the buildings design. Landmark sites exist where a landmark building or structure is or ought to be. Heritage sites are identified as having heritage value.

☐ Heritage Sites

3.2.11 Build-to Line

Build-to lines indicate the required placement of the front of a building to ensure an appropriate street or open space enclosure. The objective of a build-to line is to maintain a consistent setback and to create a continuity of buildings along the edge of a street. Where the traditional grid exists, the streets are well framed by buildings placed at desirable build-to lines

■ Setback
□ Built to lot lines

3.2.12 Tree Canopy

Downtown Moncton has an extensive tree canopy, with the exception of the City Centre and the riverfront area. The City Centre is surprisingly devoid of street trees, resulting in a significant absence of a tree canopy.

3.2.13 Brownfields

Brownfield sites are industrial or commercial sites that have been abandoned or underused and are often contaminated. The major brownfield areas in Moncton present significant re-development opportunities that can serve to frame and contain Downtown.
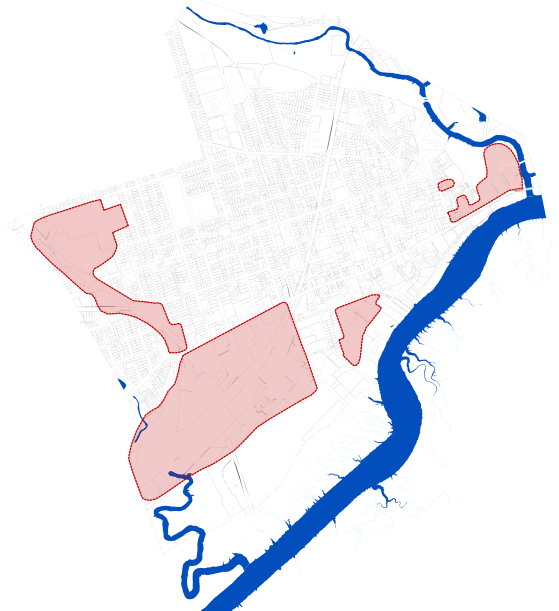
3.2.14 Public Spaces and Civic Buildings

Public spaces and civic buildings often serve as orienting functions/ uses within Downtown, and as such, they should be linked with clear pedestrian amenities such as sidewalks, crosswalks and signage.

4
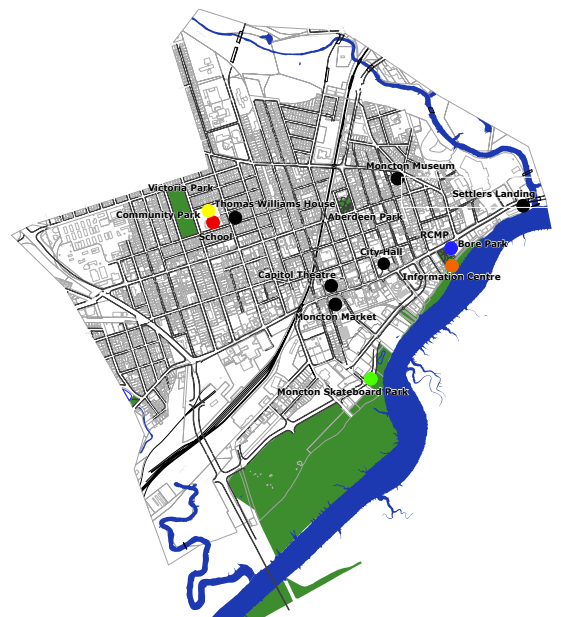
GUIDING PRINCIPLES

Identity and Culture

Partnering

Heritage

Role and Function

Environment

Quality Matters

Movement

Process

Connectivity

Metro Context

4. Guiding Principles

The following guiding principles for the Development Vision for Downtown Moncton were created in working groups during the Symposium.

Identity and Culture

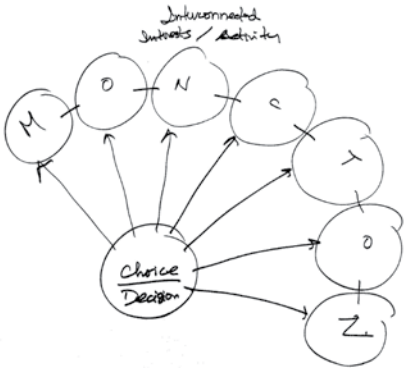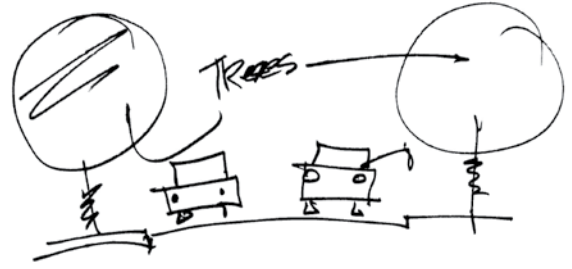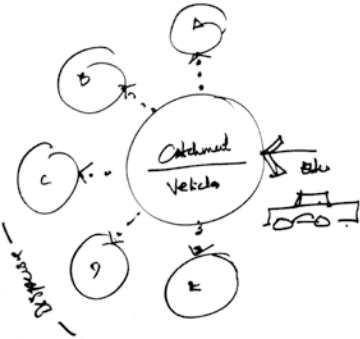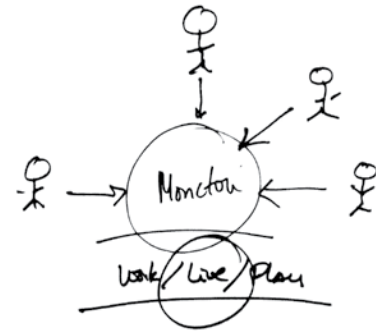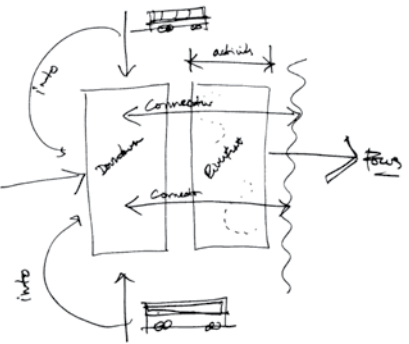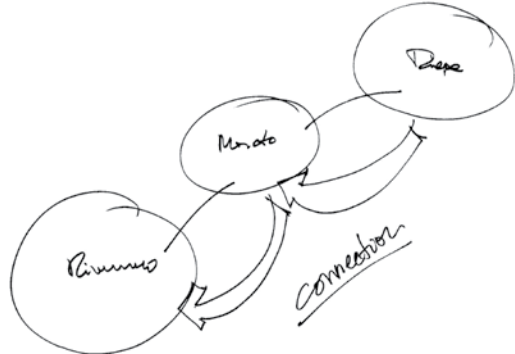Downtown Moncton will respect, promote and seek to enhance the cultural diversity of the City as a whole. It will recognize the unique character of specific areas/neighbourhoods, and will strive to create strong identities where they do not already exist.

Heritage

The existing natural and built form heritage character of Downtown Moncton will be maintained, enhanced and promoted.

Environment

Downtown Moncton will be a green environment that both respects and celebrates its natural assets, particularly the Riverfront. It will continually seek to enhance its "green" infrastructure.

Movement

Downtown Moncton will be an environment that is designed for, and promotes, an integrated multi-modal transportation (walking, cycling, vehicular) network.

Connectivity

Downtown Moncton will be an environment where uses, open spaces and circulation networks (trails, sidewalks, and streets) will be connected to facilitate ease of movement between all areas (neighbourhoods).

Partnering

Downtown Moncton will be an environment that welcomes development that is in keeping with its Vision.

Role and Function

Downtown Moncton will be designed to integrate a diverse mix of cultural, civic, business, retail and residential uses. In doing so, it will aspire to promote gathering, and to be a safe environment.

Quality Matters

Downtown Moncton will be an environment that demonstrates attention to detail and a commitment to architectural quality. All urban features will contribute to the beauty of Downtown.

Process

The Vision for Downtown Moncton needs to be understood for its importance to the entire City. City-wide decisions will be evaluated in relation to the Vision and should seek to affirm the Downtown Vision.

Metro Context

Downtown Moncton will recognize the value of collaboration between Dieppe and Riverview, and will seek to engage in initiatives that serve to strengthen all three municipalities.

5

THE VISION

sustainable

pedestrianized

fine-grained

trees, trees, trees

5.1   10 Big Moves

1 **Support the long term environmental and economic viability of Downtown**

Decisions made today must sustain the long term quality of Downtown and its natural environment.

A sustainable form of urban development ensures that the use of resources and the environment today does not damage prospects for use by future generations, i.e. the ability to provide for the needs of Moncton's current population does not impinge upon the ability of future generations to provide for themselves.

A well-used, walkable Downtown that respects natural systems such as the river bed and the creeks will minimize the ecological impact of urban living. By making Downtown an attractive and viable choice for people to live, work and play, Moncton's growth will focus inward rather than sprawl outwards. Our best option to achieve an environmentally sensitive solution for human settlements is a healthy and successful Downtown.

From an economic perspective, focusing on Downtown is about both making the best use of public investments (e.g. roads, infrastructure, and services that already exist) and maximizing the potential return (e.g. enhanced value, economic spin-offs, tourism). A sustainable approach entails making the right choices today that will allow resources to be replenished, wealth to be created, and the promotion and maintenance of business and community development strategies that lead to a better business environment in the future.

From a social perspective, the evolution of Downtown Moncton is about creating new ways to live and prosper while ensuring an equitable, healthy future for all residents.

2 **Pedestrians first**

A quality pedestrian realm is a priority that must permeate all aspects of development, public investment, policy, and event programming. The extent to which people are able to walk in a comfortable, appealing and safe fashion, will ensure that Downtown is successful and animated.

This notion is manifest at the larger scale by enhancing a network of streets, sidewalks and trails, and at the more intimate scale by crafting guidelines to manage growth and by designing special pedestrianized areas.

3 **Re-introduce the fine grain grid network of streets and blocks**

The fine grain street network that exists north of Main Street will be extended to the area south of Main Street. Existing streets that dead end at Main Street will be extended to the area in the south such as Harris Avenue, High Street, and Weldon Street. New east-west streets and pedestrian connections will be introduced to create vital links between the Jones Lake Neighbourhood and Downtown.

This will have the effect of connecting the different parts of Downtown together, drawing from the cherished neighbourhoods a historical street pattern that can inform new large developments. Pedestrians will be able to move freely along the railway lands and towards the riverfront, with the same ease as taking a stroll through Victoria Park.

4 **Trees, Trees, Trees**

Trees will be introduced along all Downtown streets, to enhance the streetscape and strengthen the pedestrian environment. Street trees provide both visual interest along the street and act as a buffer between pedestrians and vehicular traffic, making the street a safer place for walking. A significant amount of trees will also be introduced along the riverfront to enhance the area for recreational purposes and to stabilize the eroding shoreline.

connected

residential

public

unique

grand

Downtown

5 Re-connect with the Petitcodiac River and Hall's Creek

New development along the riverfront will be oriented towards the river, rather than away from it. New connections will be created that will not only provide physical connections with the River, but will also provide a visual connection that will draw people to the area. An example of this is the creation of a pedestrian connection between the end of Steadman Street South and the Petitcodiac River that ends with an observation/ look-out point, reconnecting this City with the river.

To the east, the fine grain grid will be extended to the area east of Mountain Road and King Street to create vital linkages between Downtown and the area east of Hall's Creek. Buildings should be built to face the creek along a newly introduced grid network of streets. Pedestrian paths connecting both sides of the creek will be introduced to enhance connections.

6 Introduce a significant residential population Downtown

Introducing a significant amount of residential units creates the critical mass needed to support retail and services, and encourages these businesses to locate Downtown. In addition to supporting Downtown retail and services, a significant downtown residential population will make streets safer and will help address traffic congestion. Having a large residential population Downtown creates an animated street well after offices and businesses have closed, making it a safer environment for people by having other people on the street. Residential development can occur, for example, on the north side of Assomption Boulevard and along St. George Street. Allowing the option to live near work will increase pedestrianism downtown

7 Enhance and increase public spaces Downtown - Church Street Plaza, Train Station Plaza, Market Square

New public squares and plazas will be created and existing ones will be enhanced. There is an opportunity to create several new public squares and plazas Downtown. These civic spaces will increase the significance of areas such as the train station and the intersection of Church/ Queen Streets. These areas can be finished with different materials such as unit-pavers or stone to clearly identify them as distinct places within Downtown. Civic uses are

important to the health of any downtown. They provide spaces for residents to interact and give people another reason to come Downtown.

8 Build upon the unique cultural and landscape heritage

There is a unique cultural and landscape heritage in this City and it should be celebrated. Unique buildings, unique landscapes, and unique districts are, and should be, an urban identity that is a source of pride.

There is no place like Moncton. The history is unique – the Vision must correspond. Overtime, recuperating and enhancing the heritage resources will prove to be a source of inspiration and character for Downtown.

9 Grand Avenues

All streets in Moncton should be grand – from the small scaled residential streets, to larger avenues with substantial vehicular traffic. All streets must work well for cars, and provide a superb pedestrian environment. Noticeably, Vaughan Harvey and Assomption Boulevard will be transformed into Grand Avenues. Trees and landscaping will be planted on both sides of the street and along the centre median to elevate the streetscape and enhance the pedestrian realm. Buildings will be built to face the street, creating a sense of enclosure and building the image of a grand avenue. Buildings will be built on the north side of Assomption Boulevard leaving the south side for recreational uses, ensuring the riverfront remains publicly accessible and visible.

10 Let downtown be Downtown

The Downtown has historically been a regional destination, the centre of tourism, as well as commercial activity and business for the region, and it must continue to remain as such. Significant uses, including civic, commercial, entertainment and residential, should continue to be directed to locate Downtown to enhance the existing services and amenities that exist, strengthening its role within, and beyond, Moncton.

This Vision entails focusing on Downtown as a city-wide and region-wide asset and resource.
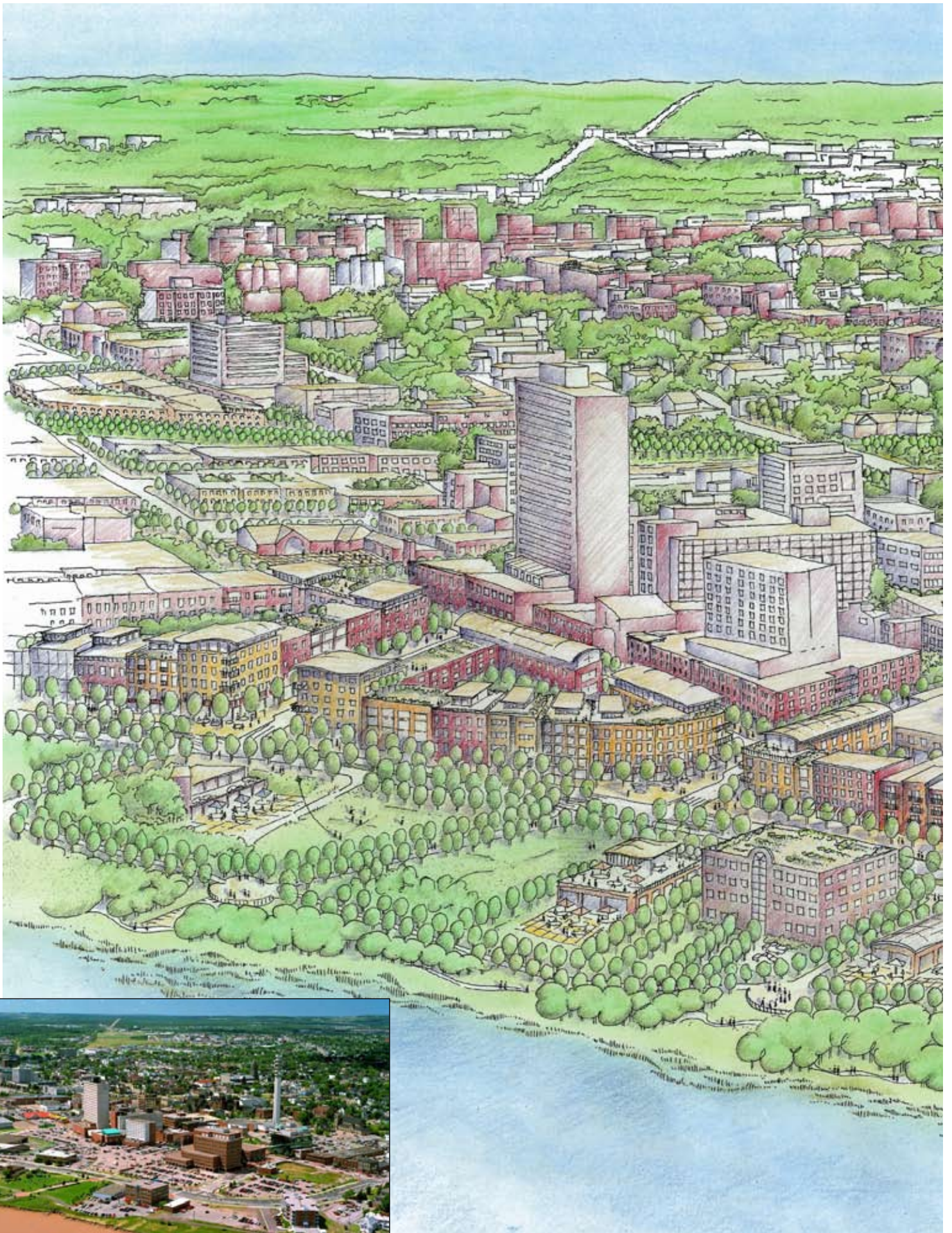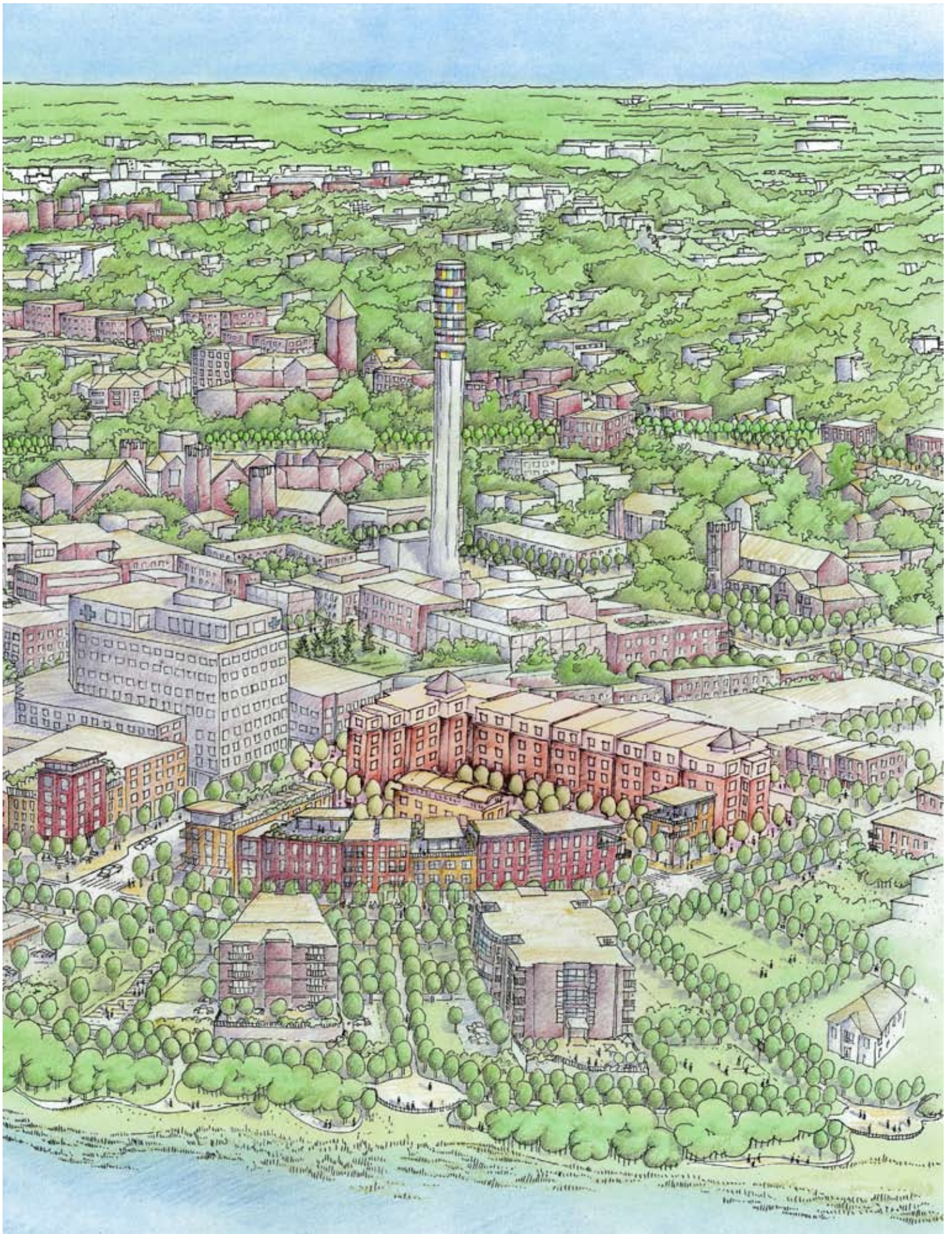
Pedestrian connections to the north and to the University should be strengthened and beautified

Add greenery to all streets

Support and integrate all residential neighbourhoods

Reintroduce the street grid as large sites are decommissioned or redeveloped

As new areas are developed, consider opportunities for new parks and public gardens

Revive St. George Street as a main street with a mix of uses: residential/commercial

Use the intersections of the street grid with the rail corridor as opportunities for new public spaces and a renewed streetscape

New buildings on Vaughan Harvey Boulevard should face the street, and help build the image of a Grand Boulevard

Highlight the Subway as an important urban landmark and gateway. Consider a design competition and art focus

Gordon Street should be continued across Vaughan Harvey Boulevard to de-isolate the west end for vehicles and traffic

Reorient Highfield Square and ensure all new developments face the street

Consider a "Federal Square" as part of the identity of Main Street west of the subway

Bury the power lines along Assomption Boulevard

Extend the downtown grid that exists north of Main Street into the underutilized land between Main Street and Assomption Boulevard

Celebrate the Train Station by creating a public plaza

Transform Vaughan Harvey Boulevard into a Grand Boulevard

Main Street should continue west as a mixed-use corridor, serving both as a neighbourhood main street and part of a regional amenity

Prohibit construction south of Assomption Blvd

Create a gateway into the downtown at the intersection of Vaughan Harvey Boulevard, Main Street and Assomption Boulevard
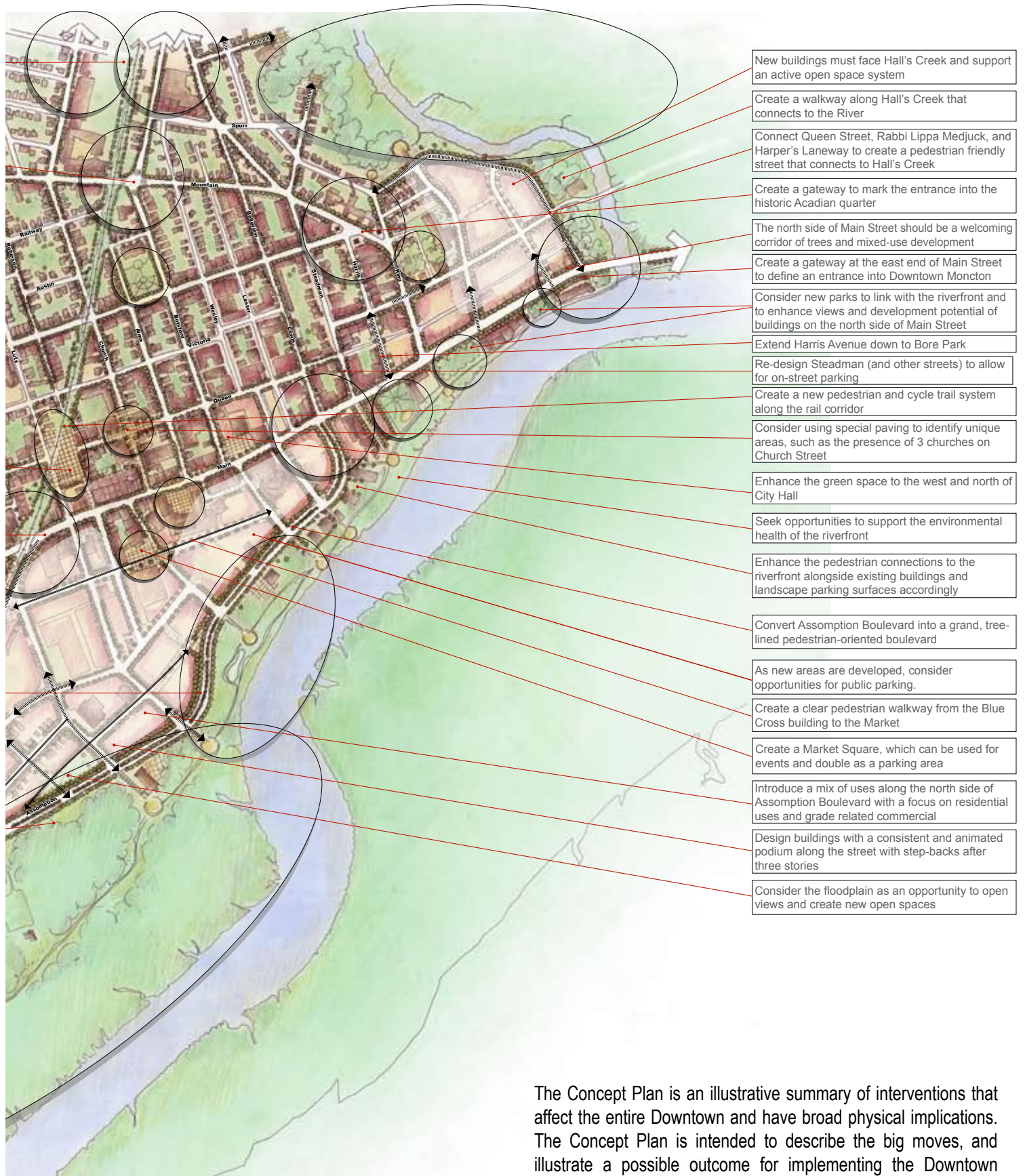
Plant 5,000 Trees!

Unearth the creek and integrate with the open space trail system

Extend Assomption Boulevard west of Vaughan Harvey Boulevard

NORD NORTH

5.2 The Concept Plan

New buildings must face Hall's Creek and support an active open space system

Create a walkway along Hall's Creek that connects to the River

Connect Queen Street, Rabbi Lippa Medjuck, and Harper's Laneway to create a pedestrian friendly street that connects to Hall's Creek

Create a gateway to mark the entrance into the historic Acadian quarter

The north side of Main Street should be a welcoming corridor of trees and mixed-use development

Create a gateway at the east end of Main Street to define an entrance into Downtown Moncton

Consider new parks to link with the riverfront and to enhance views and development potential of buildings on the north side of Main Street

Extend Harris Avenue down to Bore Park

Re-design Steadman (and other streets) to allow for on-street parking

Create a new pedestrian and cycle trail system along the rail corridor

Consider using special paving to identify unique areas, such as the presence of 3 churches on Church Street

Enhance the green space to the west and north of City Hall

Seek opportunities to support the environmental health of the riverfront

Enhance the pedestrian connections to the riverfront alongside existing buildings and landscape parking surfaces accordingly

Convert Assomption Boulevard into a grand, tree-lined pedestrian-oriented boulevard

As new areas are developed, consider opportunities for public parking.

Create a clear pedestrian walkway from the Blue Cross building to the Market

Create a Market Square, which can be used for events and double as a parking area

Introduce a mix of uses along the north side of Assomption Boulevard with a focus on residential uses and grade related commercial

Design buildings with a consistent and animated podium along the street with step-backs after three stories

Consider the floodplain as an opportunity to open views and create new open spaces

The Concept Plan is an illustrative summary of interventions that affect the entire Downtown and have broad physical implications. The Concept Plan is intended to describe the big moves, and illustrate a possible outcome for implementing the Downtown Vision.

5.3 Urban Structure

Urban structure is the arrangement of land uses (e.g. parks, residences, offices, business) and urban infrastructure (e.g. roads, bridges, trails) where people engage different aspects of their everyday life.

A previous section presented the existing urban structure of Downtown. The following section presents the proposed changes, necessary to implement the Vision.

5.3.1 Improve links from Downtown to the Greater Moncton and beyond

Precisely because Downtown is a city wide imperative, it is important that people from Greater Moncton are able to reach Downtown, and conversely that people who live in the urban core are able to connect with ease to the broader region.

Connections must occur across all modalities: vehicular, pedestrian, cycling, and transit. This way people will always be able to opt for the most convenient mode, at any given time, under any given weather condition. Connections must also take place through the extension of the open space system, through programming, cultural events, and the bridging of identities and initiatives.

5.3.1.1 Towards the University

This is perhaps one of the most important connections to establish. Currently the Université de Moncton has a limited presence and relationship with Downtown. This must change. Students and faculty should come Downtown often, they should be able to live, study, and conduct a big part of their life Downtown. This entails making good road connections towards the University (vehicular and pedestrian), but also fostering an environment where students can thrive, and actively seeking partnership opportunities for the University to establish a presence Downtown.

Enhancing a trail system along Hall's Creek (from the University

to the Petitcodiac River) and further pedestrianizing the bridges (Church and Botsford) would be a good first step.

5.3.1.2 Towards Jones' Lake

Jones Lake, the West End and the Old West End, are neighbourhoods that are significant users of Downtown amenities and commercial base. The flow of people between these two areas can only increase when they come closer together through new developments (e.g. rail lands) and a rejuvenated Vaughan Harvey. The Downtown Vision identifies several locations where road connections ought to take place:
  • Extend Main Street west as far as the Causeway.
  • Extend Assomption Boulevard west, creating a distinct edge to the park system.
  • The continuity of Gordon Street should be restored across Vaughan Harvey to increase movement options.
  • Vaughan Harvey should become a Grand Boulevard that is attractive to pedestrians.
  • Jones' Creek should re-surface as much as possible, connecting pedestrian trails to the Petitcodiac River and the overall park system.
  • The intersections of Vaughan Harvey with John, St. George, and Main should be given special attention as gateways, where the comfort and safety of pedestrians crossing Vaughan Harvey should be paramount.
  • Opportunities for new east-west streets and pedestrian connections entering and/or crossing Vaughan Harvey should be explored with each new development.

5.3.1.3 Towards Hall's Creek

The history of Hall's Creek had been one of an industrial waterway. Now, Hall's Creek and adjacent green spaces should be viewed as an important part of the open space network, connecting the Petitcodiac River with neighbourhoods in both Moncton and Dieppe.

To this effect, every opportunity should be capitalized to:

- Make pedestrian connections from the urban edge into the Hall's Creek's trail system
- Enhance views into the creek
- Turn buildings and the active uses within buildings, towards Hall's Creek

5.3.1.4 Towards other neighbourhoods

Connections to Greater Moncton should be enhanced. This includes Mountain Road and all north south streets. This also includes the parks and trails systems, such as a new trail along the rail corridor.

Precisely because Downtown is a city-wide imperative, it is important that people from Greater Moncton are able to reach Downtown, and conversely that people who live in the urban core are able to connect to the broader region with ease.

5.3.1.5 Towards Dieppe and Riverview

Great synergies have yet to emerge between the three municipalities. While maintaining the individual character of each city (and also the distinctiveness between neighbourhoods and districts), enhancing connections towards these cities can only enhance the success of the whole region.

Connections towards Dieppe should be established along the open space system, through transit and the active transportation network. A unique opportunity resides in shifting the perception of Hall's Creek from being a boundary between cities to it becoming a heart, a centre (much like a neighbourhood park), and a focal point for an urban environment that straddles both cities and includes the University.

Towards Riverview, strides have been made to utilize the Petitcodiac River as a worthwhile seam between the two communities, connecting through trails, roads, a new bridge and open views.

Downtown Destinations: Red are existing, blue are envisioned.

Existing Regional Destinations

Proposed Destinations

5.3.2 Augment destinations and landmark sites

Promote multiple destinations for a variety of users, dispersed throughout Downtown. Downtown is, and should be, used by a wide variety of people for different reasons, at different times of the day, month and year. Maintaining a wide range of destination sites with broad appeal and year-round use for local residents and tourists alike.

Frontages with potential for primary Downtown landmark structures

Frontages with potential for secondary landmark structures

Gateways with potential for landmark structures

Areas with potential for landmark open spaces

Open spaces with potential for landmark elements

Opportunities for new significant landmarks

View corridors and view terminus: Arrows denote view corridors that must be protected and enhanced. Circles denote view terminus.

Legend:
- View Corridor
- Downtown-Wide View Terminus
- Local View Terminus

5.3.3 Sustain and enhance view corridors and view termini

View corridors have a critical role to play in orienting people towards significant features, and highlighting the presence of landmarks. Within Downtown all identified view corridors take place along the public realm, a street or a park. This implies that pedestrians can walk along the view line to reach the object of their viewing.

The width of the view corridor depends on the focus of the view. The focus of the view may be a single element, involving a building such as City Hall, which would result in a narrow corridor, or an open view such as the riverfront, which would result in a wide corridor.

For Downtown the most important views are towards the Petitcodiac River and towards Hall's Creek. Maintaining a clear view towards both these riverbeds will assist in in orienting people within Downtown, and will also assist people in reaching the open space network with ease. It will also give buildings in the view corridor a wonderful view.

Other important view corridors are focused towards landmark buildings such as the churches and City Hall.

View corridors should be designed by:
- maintaining the corridor clear of any projections that would obstruct the view; and
- framing the view with built elements, such as a continuous street wall of building façades.

Locations for gateways: Circles denote street intersections and areas where a gateway ought to be designed

5.3.4 Build recognizable Gateways

A gateway is most often a landscape element or design feature intended to signify entrance to a distinct area, usually in places where a new character or sense of identity should be recognized. There are several gateways to Downtown and within Downtown proposed.

Gateways can be designed as part of buildings (e.g. a turret), adjacent to buildings (e.g. a large sculpture), as part of the landscape (e.g. a bridge), and may include a graphic signifier (e.g. a logo or a "welcome to Downtown" sign). But most importantly, gateways must be clearly identifiable as such, connoting a transition between two parts of the city. They should heighten a sense of entry to, or exit from, a distinct part of the city.

Heritage Area Studies: areas that should be studied for their heritage attributes.

5.3.5 Study and Plan Heritage Area Studies

Heritage Area Studies (HAS) are an important tool to assist in the preservation of exceptional neighbourhoods, streetscapes, and architectural features that are emblematic of specific eras and locales. Different from designating individual buildings for their heritage attributes, an Heritage Area Study focuses on an entire district, a collection of buildings, streets, open spaces and landscapes.

As part of this study, a wide range of stakeholders, community members, professionals and stakeholders provided input on what heritage characteristics are valued, and what set of guidelines should be put in place to guide the evolution of the district in support of the heritage character.

The benefits of designating an area as a Heritage Area are many. Members of the community can preserve local values through the built form, ensuring that treasured elements within the community are protected over the long term. Home owners, business owners, and the municipality itself all stand to enjoy economic benefits from the preservation of a vibrant heritage district, whose unique cultural value attracts additional investment and visitors to the area. Importantly, designation can ensure that a district's heritage attributes are not degraded by growth and change; but rather managed, preserved and promoted effectively over time.

For Downtown, the process of undertaking individual Heritage Area Studies and the subsequent creation of plans and guidelines can serve to:
- ensure that community members continue to be engaged in the planning process;
- develop an understanding and appreciation of heritage resources;
- recognize and commemorate the values that can sustain a sense of place into the future;
- contribute, through enhanced awareness and defined policy, towards the development of a rich physical and cultural environment;

- provide clarity and certainty into the future for property owners, businesses and residents alike; and
- provide a framework to manage change.

Commercial Corridors
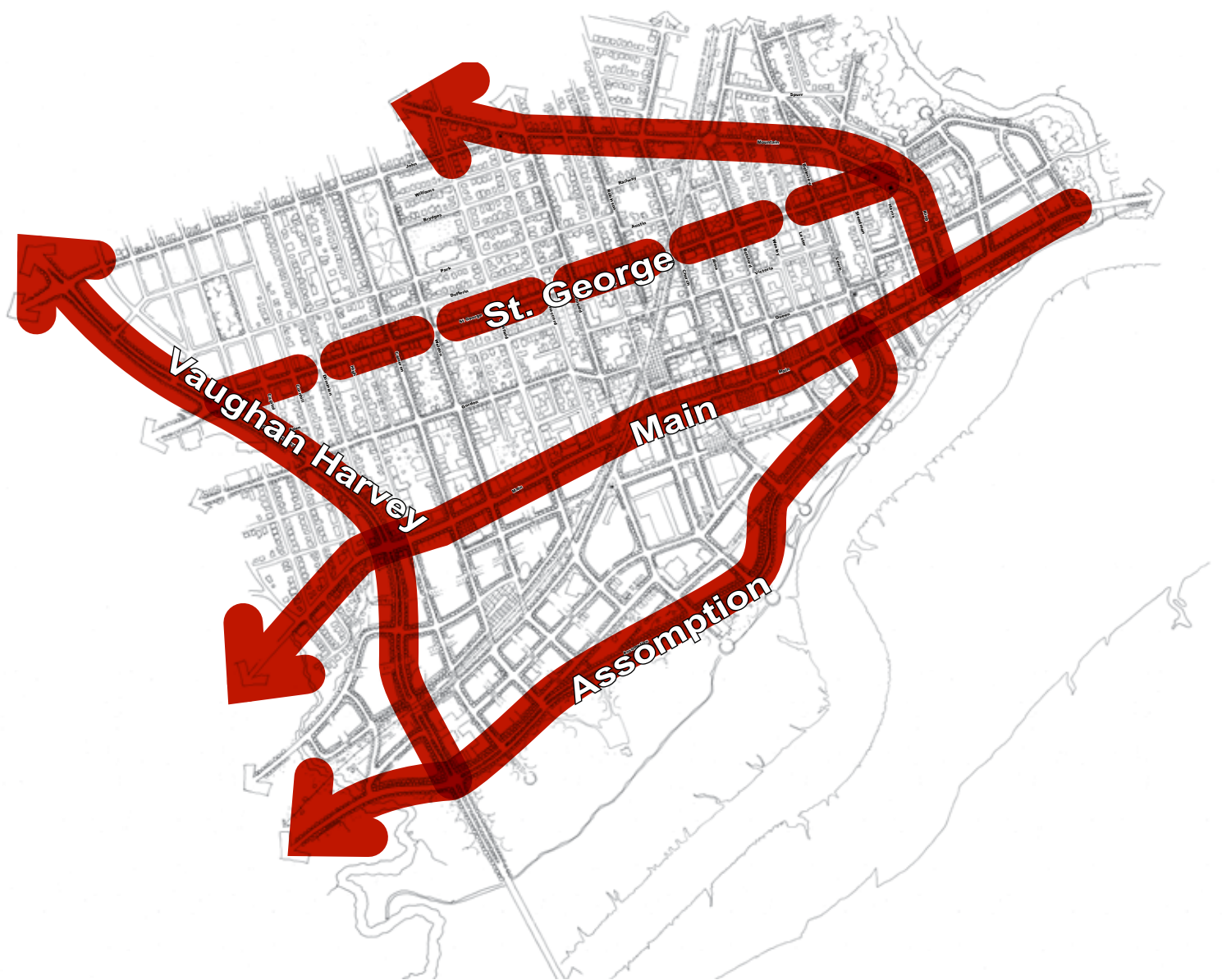
5.3.6   Integrate Uses

Imagine being able to live Downtown, walk to work, buy groceries on the way home, visit a friend, go to the doctor, use the local school, and attend a street festival on the weekend – all within walking distance.

The integration of uses is about supporting all aspects of everyday life within Downtown. To the same extent as Downtown is used by residents of the entire region, it must also be a successful neighbourhood. Rather than have vast single-use areas, which often entail car dependency, Downtown will be enlivened by having a variety of uses attracting a variety of users during different times of the day, and during different times of the year.

Encourage integration of uses in a variety of forms:
- Horizontal integration: along a street and within a street. Flowing from, and between, a series of residences, to offices and retail.
- Vertical integration: within a single building. For example, retail at the street level, offices on the second and third levels, residential above.
- Flexibility: buildings that can change use overtime, and be flexible to evolving conditions. An example is a live-work unit, where residents have the option to open a studio, a home-office, a small store.

5.3.6.1 Support diverse residential uses

Not only are residences essential to the health of downtown, it is also crucial to have variety of residential types: families, students, seniors, young-professionals, and a spectrum of income brackets.

A creative exploration of diverse residential types will also ensure that all parts of Downtown are well used and animated. Opportunities for student housing, for example, should be explored above storefronts, in secondary-units, basement apartments, and as shared facilities.

Over the years Downtown has maintained a strong residential base. Many opportunities exist to enhance this: from developing underutilized sizes to refurbishing underutilized parts of buildings (e.g. upper level units on Main Street).

Building a strong residential base will be complimented by enhancing connections to adjacent neighbourhoods.

5.3.6.2 Commercial corridors / areas

While employment and commercial uses should be integrated throughout Downtown, several areas are identified for the concentration of retail and offices. These are not intended to be limiting, but rather to focus investment, define the character of the pedestrian environment, and allow businesses to thrive from proximity to one other.

Identified main commercial corridors/areas include:
- Main Street
- St George Street
- Assomption Boulevard
- Vaughan Harvey Boulevard
- The Core
- King / Mountain Road

5.3.6.3 Re-development opportunities

Several areas in Downtown offer significant redevelopment opportunities.

Small
An extra floor on an existing building, a secondary unit at the back of a property and building conversions, are examples of small scaled interventions that can have a cumulative effect.

Within Downtown, the development potential of many properties has not been fully realized. Many buildings remain either vacant or are not well used. As these small opportunities come to fruition,

Areas of large re-development opportunity

Areas with opportunities for infill

Areas of large re-development opportunity

incremental residential infill will evolve in a way that is integrated and seamless.

Medium

Individual properties abound in Downtown that are either vacant or under-utilized. Developing these properties should be seen as an opportunity to stitch different parts of Downtown together, creating a complete urban environment. On Main Street, for example, each new building should help complete the integrity of a continuous commercial main street with an uninterrupted pedestrian environment.

Large

Large properties and land assemblages can offer unique development opportunities. These sites have the potential to make a significant contribution to their context. As opposed to smaller sites, large developments may become important destinations within Downtown, by concentrating public uses such as a theatre, a community centre or an office complex.

One good development of significance can transform an area, and serve as a catalyst for the neighbourhood.

Opportunities for public parking facilities should be considered in all large developments.

Extra-large

As Moncton has shifted away from the port and railway industries, significant tracts of land have opened for re-development. More than simplify transforming a streetscape or a neighbourhood, these areas have a critical role to play in the consolidation of entirely new urban identities within Downtown. They also have the potential of introducing a significant population Downtown.

For these areas it will not suffice to generate a site specific plan. Any development within these areas must be based on a comprehensive Master Plan, including all properties within the precinct and surrounding context, which establishes an understanding of the character and prospects of both short-term and long-term developments.
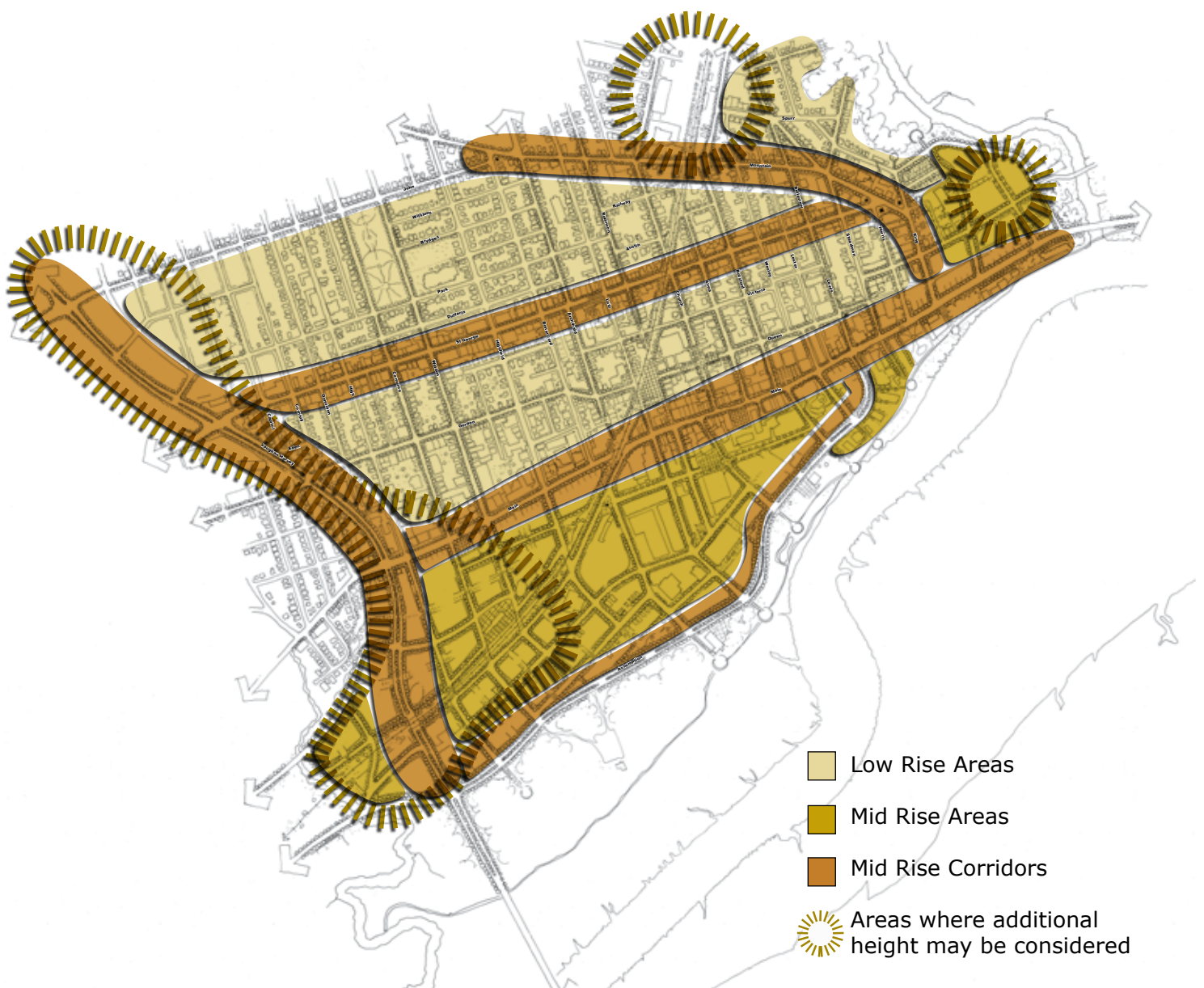
These areas include:

- The railway lands, located east of the tracks, south of Main Street, north of the Petitcodiac River and extending west past Vaughan Harvey towards Jones' Lake.
- The East End, located north of Main Street and extending west from Hall's Creek.
- The military lands, located on Vaughan Harvey, as they are transformed in the long-term.
- Assomption Boulevard, in its new alignment, with the significant development opportunities created on its north side.

5.3.6.4 Neighbourhood centres and catalysts

Some sites function as neighbourhood centres, given their prominent presence, their location relative to other uses, and the intensity of uses and programming within them. Downtown already has many such centres that provide a focal point for activity for both local residents and visitors, such as the Capitol Theatre, the Aberdeen Cultural Centre, City Hall, schools, parks and plazas. In order to maintain their effectiveness, and for the development of new centres, consider:

- A dispersed approach. By locating each centre at an energy distance from each other, energy can build-up in the space between them. For example, if a variety of entertainment venues are spread out along Main Street, the whole corridor will develop with the supporting restaurants and amenities.
- Strategic phasing. Not all centres will thrive at the same time. The distance linking centres may take some time to develop. Conditions must be ripe for a centre to function as a catalyst for growth and liveliness.

Legend:

- Low Rise Areas
- Mid Rise Areas
- Mid Rise Corridors
- Areas where additional height may be considered

Height development pattern

5.3.7 Consider Height

Downtown already has a variety of building types that offer a range of building heights – from the 3-4 story podium of Main Street, to the high-rise presence of Assomption Place and the Delta Beauséjour. Several recent developments offer a mid-rise typology ranging between 5 to 10 stories.

The question of height must be considered hand-in-hand with aspects of density. It is one of the objectives of the Downtown Vision to significantly increase the density of Downtown in order to enhance the quality of urban living and the pedestrian realm, and to create a more sustainable approach to city building.

As existing Downtown neighbourhoods are infilled, and new areas developed, consider:
- Generally maintaining current heights in the heart of historic areas, as may be confirmed through Heritage Area Studies.
- Maintain the height character of existing low-rise neighbourhoods, north and south of St. George Street.
- Provide a transition of height in buildings located near different height character areas (e.g. near a low-rise neighbourhood).
- Protect important pedestrian areas from shade and shadow impacts.
- Protect the windows of residential areas from shade and shadow impacts.
- Protect views to, and the profile of, landmark buildings such as the church steeples on Church Street.
- Consider height as an architectural tool. Draw attention to new landmark elements such as gateways and new civic buildings using height.
- Allow for greater heights in new neighbourhoods as appropriate, including the railway lands and Vaughan Harvey Boulevard.
- Don't under-build! One story buildings on Main Street, for example, only detract from the dense urban character and mix of uses on the street. A minimum of two stories should be built facing all streets in Downtown.

A framework for building heights is identified in the adjacent map, and includes:

Low-rise areas
- Residential pockets north of Main Street, and north of St. George Street.

Mid-rise areas
- The Core
- The Railway lands
- The East End
- A small existing pocket south of Assomption Boulevard and Main Street

Mid-rise corridor
- Main Street
- Vaughan Harvey Boulevard
- Assomption Boulevard
- St George Street
- King Street and Mountain Road

Areas where high-rise may be considered
In some conditions a high-rise building or structure may be appropriate. These cases must be evaluated individually, to determine their contribution to the urban space, and the impact on adjacent uses. A building that excels architecturally has a positive presence within its context - by contributing to the character of the area and not having adverse wind and shadowing impacts - and will be in keeping with the Vision for Downtown Moncton.

Street network

The Grand Boulevard – Vaughan Harvey

Mixed Main Street – Main Street

Mixed Main Street – St. George Street

Rail corridor and pedestrian trail

The Scenic Drive – Assomption Boulevard

Grand Street - Mountain Road

Mixed Neighbourhood Street

5.4 Circulation and Connections

The circulation and connections network is based on similar principles as the open space network, providing a well connected system of streets and paths, which facilitate ease of movement throughout Downtown Moncton.

Permeability is at the forefront of each component of the circulation network. Permeability is about ensuring that pedestrians (as well as all other modes of transportation) are able to move freely both through spaces and around them. Connections should remain unobstructed so that pedestrians have a diverse range of options available to them in deciding the best route to get to their destination.

The following is an overview of the different components that make up the circulation and connections network.

5.4.1 Street Network + Street Hierarchy

The street hierarchy and network is defined by the existing or anticipated vehicular patterns of traffic for streets Downtown. The street network and the classification of streets help define appropriate approaches for streetscape and built form considerations, depending on the intended function and character of the street.

The Grand Boulevard – Vaughan Harvey

Vaughan Harvey Boulevard serves a double function. The first one, which currently is being achieved successfully, is moving vehicular traffic efficiently from the north part of the City to the new Riverview bridge. The second function, which is the opportunity expressed in this Vision, is to become a focus for higher order developments alongside magnificent pedestrian connections.

Vaughan Harvey Boulevard is wider than any other street in Downtown. It therefore presents an opportunity to become a Grand Boulevard, more than any other street Downtown.

The multiple lanes of traffic should be enhanced ith extensive landscaping, noticeable gateways and superb pedestrian walkways and cycling paths.

Vaughan Harvey Boulevard is the western seam that links Downtown to Moncton's west end. As such, it must function as a seam, not a barrier. East-west connections (for vehicles and pedestrians) must be plentiful, attractive and safe. A central median should be well landscaped and offer a refuge for pedestrians crossing Vaughan Harvey Boulevard.

New buildings should front directly onto Vaughan Harvey Boulevard. These buildings should take full advantage of the boulevard identity and make the pedestrian connections better used by adding active uses. For example, at the intersection of Vaughan Harvey Boulevard and Main Street, all four corners should develop buildings directly related to the sidewalk. This will increase the capacity for Main Street to extend the "main street" character and commercial function of the street, westbound.

On-street parking on both sides of the street will serve new uses that locate on Vaughan Harvey Boulevard, and will provide a buffer between moving vehicles and pedestrians. If and when traffic demand increases, on street parking can be restricted during peak times.

Grand Streets

Grand streets are important points of entry to Downtown, for cars and pedestrians. Because they concentrate all modes of movement they have an increased profile that can translate into a greater concentration of buildings. On these streets a mixed-use environment can flourish successfully. An office building, for example, will be highly visible and accessible. These include:
- Botsford Street
- Mountain Road
- St. George Street west of Vaughan Harvey Boulevard

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These Grand streets should be characterized by larger buildings, reflecting the broad functional importance of the street. Streetscaping treatments and the manner in which buildings interface with the street should be highly sensitive to the comfort and interest of the pedestrian. Architectural treatments should be appropriately scaled to make an impression on people moving in cars as well as walking.

Given their significance as entry points into Downtown, they should also be considered as gateways, especially at all major intersections.

On-street parking on both sides of the street will help disperse the demand for parking spaces throughout Downtown, and increase the viability of a range of uses.

The Scenic Drive – Assomption Boulevard

Assomption Boulevard should be unlike any other street in Moncton. Nowhere else does the city meet the water in such a grand fashion.

Assomption Boulevard functions as an alternative route for vehicles, bypassing a portion of Main Street. Most importantly it will create a new interface between the city proper and the riverfront, and provide a central spine for pedestrian movement along the urban edge. The character of this area is described in more detail in Section 6.

While the north side of Assomption Boulevard is lined with mid-rise buildings, these should achieve enough density to support a public realm that is active year-round. The south side of Assomption Boulevard will become an enhanced riverfront park. The street will have the capacity to concentrate intense pedestrian activities, while providing vehicles, cyclists and transit witha connection to Main Street.

On street parking on both sides will serve both the commercial and residential uses to the north and the recreational open spaces to the south.

Mixed Main Streets – Main Street and St. George Street

Main Streets are similar to the grand streets in their need to ensure the comfort and visual interest of the pedestrian. What differentiates main streets is the focus on retail activity and the resulting reinforcement of the human scale - a vibrant street life including sidewalk cafés and 'spill-out' activities, which are of paramount consideration. Human scale is also an important consideration in the massing and heights of the buildings along these streets, as they cast shadows and shape views.

Two main streets cross Downtown from east to west: Main Street and St. George Street. Each one is described in detail in the section on precinct areas.

On-street parking along both sides of the street will serve motorists by providing an extensive and dispersed parking strategy, and will also serve pedestrians by creating a safety buffer between the street and the sidewalk area.

Neighbourhood Street

Neighbourhood streets support residential areas and reinforce the existing residential scale. Where appropriate, traffic calming measures may be implemented to control the speed of vehicles. Many cherished examples exist for neighbourhood streets in Downtown, such as the blocks surrounding Victoria Park. As new developments extend the urban pattern of Downtown (noticeably into the railway lands) the good examples should carry through.

On-street parking on one or both sides of the street will serve local residents, absorb and disperse some of the parking needs throughout Downtown, and support a safe pedestrian scale to the street.

5.4.2 Pedestrian Movement

Downtown will be experienced, to a great extent, by walking.

The success of Downtown will be tied to the opportunity for pedestrians to move efficiently within Downtown, and from Downtown to surrounding neighbourhoods. One of the many factors that play into a pedestrian's decision whether to walk is the amount of time it takes to get to destinations. An effective circulation network needs to provide as many options for getting to destinations in the shortest distance possible.

The most basic element of an efficient circulation network and in creating pedestrian friendly spaces is providing sidewalks on all streets. This visually communicates to pedestrians that walking is welcome, safe and expected.

Crosswalks enhance the pedestrian experience by providing people with a sense of safety and comfort, and by reducing the speed of traffic. Further, they clearly demarcate a safe route for a pedestrian to cross; and act as a traffic calming measure. Every key four-way intersection that is signalized should have crosswalks and new ones should be added over time. Major crossings should utilize paving embedded into the asphalt.

5.4.3 Mid-block Connections

Mid-block connections are essential for creating places where pedestrians are the priority. They help create a finer-grained pedestrian network than the typical block pattern. Most of these connections usually occur between the sides of two buildings, but potential pedestrian bridges across Hall's Creek are also considered to be mid-block connections because they serve the same purpose of minimizing walking distance.

Mid-block connections should adhere to the same quality and design standards as other public spaces. Adequate lighting, appealing landscaping, clear signage, unobstructed views and uses that front on these connections are all important objectives.

5.4.4 Transit

A successful transit system is efficient, safe and fast, and is a first choice option for movement. It is a viable transportation alternative to the automobile. Convenience is the primary factor for determining whether someone will use public transit or drive. Transit should be easily accessible and provide the most direct route to destinations. Transfers should be minimized.

Currently there is a centralized transit hub on the western portion of Main Street. The transit network should be planned around multiple destinations / nodes to increase both the presence of and access to public transit. Further, transit planning must recognize the regional role Downtown plays and should therefore evolve accordingly.

5.4.5 Parking

Parking is vital to the success of any downtown. It makes Downtown accessible to both local residents and visitors. However , as the vibrancy of Downtown increases, parking management will intensify as an issue in need of careful attention and fraught with tensions. The folowning strategy recognizes thata multi-pronged approach to parking issues will provide the best results.

Reduce the need for parking

An integral part of solving parking must be creating strategies to reduce car use to both reach Downtown and to circulate within Downtown. Foremost, a walkable Downtown where people can live, work, and be entertained within walking distance is desirable. Next, non-motorized alternatives to circulate, such as a system of trails and cycle-paths should be provided. Finally, a strong transit system that is convenient and accessible year-round is fundamental.

Locate parking strategically - disperse and diversify

Different people will choose a different parking solution depending on a variety of factors such as availability, weather, cost, time, and distance. Parking is more likely to satisfy some or all of these needs if a diversity of options are available. Parking spaces should be dispersed throughout Downtown to support a diversity of destinations, rather than a single concentrated activity.

Parking should be located to minimize visibility from the street, preferably at the rear of buildings and not to the side or front of buildings. Locate parking facilities on blocks and streets in which they best serve their function without jeopardizing the pedestrian quality of Downtown and ensuring continuity of the pedestrian environment.

Provide clear signalization

A parking space only works if people can find it. Clear signage throughout Downtown is needed, especially for public parking, and given that it will, ideally, be located behind buildings.

Encourage shared-parking solutions

Shared parking is strongly encouraged between adjacent or vertically mixed uses whose peak demand is off-set from each other, such as residential and commercial uses.

New developments may incorporate municipal lots

Opportunities for integrating municipally owned parking lots into new developments should be considered within Downtown.

Maximize on street parking

On street parking can be the primary form of public parking and is often the most accessible and desirable for visitors. On street parking is especially successful for dispersing parking throughout Downtown, allowing people to park directly in front of destinations, and creating a physical and visual buffer between moving vehicles and sidewalks/pedestrians.

All streets, where possible, should have on street parking. For example, on street parking on Assomption Boulevard will provide a dispersed parking solution for people who want to access the riverfront and adjacent amenities.

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5.5 Open Space Framework

Open space connections are imperative to a downtown to make it possible to navigate on foot with ease. The open space network for Downtown Moncton is comprised mostly of lands in public ownership, often referred to as the public realm, which include all the streets, sidewalks and parks.

Opportunities exist to connect Downtown with the riverfront in a meaningful and obvious way, and to improve the open space system across the city. Main streets, when beautifully designed and well treed, become part of the open space connections. Streets connecting open spaces, such as Victoria Park to Downtown, should reflect a higher design standard.

The Open Space Framework has been generated based on the idea of 'city to river – river to city,' where visual and physical connections will be created, allowing nature to penetrate into Downtown. The Open Space Framework seeks to enhance the Riverfront and open spaces along Hall's Creek, re-integrating them back into the City and also utilizing these natural features to create vital east-west and north-south open space connections.

The Open Space Framework will also enhance existing open space amenities by encouraging pedestrian connections linking the various urban parkettes together in a thread of green. The tree canopy can be enhanced through the planting of more trees. Where there are significant development opportunities, new parks and gardens should be encouraged as much as possible.

Following is an overview of the different elements that make up the Open Space Framework for Downtown Moncton.

5.5.1 The Petitcodiac Riverfront

The Petitcodiac riverfront extends along the entire southern edge of Downtown. The area south of Assomption Boulevard should remain an open space amenity for the enjoyment of residents and should remain free from development. Maintaining the riverfront as open space will keep it accessible to the public; maintain view corridors to Downtown and the riverfront; and enhance the open space network by linking Dieppe, Riverview and Moncton along the Petitcodiac River.

Significant work has already been done to enhance the trail system along the Petitcodiac. This has started to change the perception of the riverfront from an industrial use to a recreational amenity. The challenge now is to bring the rest of the city down to the water. One way to do so is by enhancing views and pedestrian connections along well designed streets. Another is by developing a well defined and animated urban edge along the north side of Assomption Boulevard.

The absence of development on the south side of Assomption Boulevard will allow landscaping and natural growth to take place. More trees should be planted to both enhance the riverfront and stabilize the riverbank.

5.5.2 Hall's Creek

A new trail system is proposed along the creek bed, connecting the Petitcodiac River to the north part of the city, the University and Dieppe. The use of this trail and open space must be enhanced by multiplying the opportunities for pedestrians to access Hall's Creek. Views must be opened up, and new pedestrian connections should be created. Creating connections towards the creek will also serve to link the area east of Hall's Creek to Downtown.

Any new development should turn to face the creek (through doors, windows and active uses) , to contribute to an engaging open space system by having an animated street along the edge of Hall's Creek. Where possible, the edge of the creek should be lined with a public street, from which buildings can be accessed. A public street is the best way to provide public access and integrate the open space to the rest of the city.

Hall's Creek

The East End

Aberdeen

Victoria Park

The 3 Churches

City Hall

Bore Park

Assomption Plaza

Market Square

Federal Square

Central Riverfront

Station Plaza

Petitcodiac Riverfront

Jonathan Creek

Civic Spaces and Plazas

5.5.3 Civic Spaces and Plazas

Civic spaces and plazas are landscaped public spaces with a high degree of hard surfaces, generally placed adjacent to commercial or civic uses and defined by streets and buildings. They are designed for pedestrians, but in some cases they function as surface parking when they are not in active use (e.g. a Market Square). Civic spaces and plazas can accommodate a variety of uses and functions and play a vital role as district, neighbourhood and Downtown-wide focus areas. Civic spaces and plazas not only project the image of a City (i.e. Rockefeller Center in New York) but are also spaces that people have a strong connection with. Good examples already exist in Downtown: City Hall (public ownership) and Assomption Plaza (private venture).

Civic spaces and plazas contribute to community health by providing people with relief from urban living. They provide pockets where people are able to slow down their pace, engage in activities and provide people with an outlet for meaningful social interaction. They should be located at regular intervals, in areas with high pedestrian concentrations, and defined by the adjacency of active uses (most often civic, entertainment and employment uses).

Opportunities for new civic spaces and plazas have been identified in the following locations:
- The VIA rail train station
- Federal Square at the south-west corner of Main and Cameron Street
- Market Square located behind the Market between Westmorland and Robinson Street
- The area where the railway tracks intersect Robinson and Lutz Streets
- At the intersection of the 3 churches at Church and Queen Streets

5.5.4 Neighbourhood parks and amenities

Neighbourhood parks serve the broad community and come in a variety of scales and configurations. Neighbourhood parks include both naturalized and formal areas and contain a variety of activities ranging from playgrounds to athletic fields.

Neighbourhood parks should be located within walking distance to where people live. In this sense, Downtown is well balanced. From Victoria Park, to Aberdeen Park, each existing neighbourhood has a green space within close proximity. Enhancing and integrating the riverfront and Hall's Creek will only support a system of neighbourhood amenities.

To ensure neighbourhood parks are easily accessible and safe, parks should have no less than one side defined by road frontage (ideally all sides) and through-views should be clear of obstructions.

Any new medium to large scale development should integrate to the existing system of open spaces and provide additional open space amenities, such as the military base at the corner of John Street and Vaughan Harvey Blvd, when the site is re-developed.

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5.5.5　The rail corridor

The rail corridor should be integrated into the open space network and trail system, creating a green corridor that integrates the north-eastern neighbourhoods of Downtown, to the Core, towards the future neighbourhoods along the railway lands to the south-west, and into the riverfront and Jones Lake area. Landscaping the rail corridor will help soften the hard edge that currently divides Downtown Moncton into two sections. It will also provide an ideal off-the-street option for pedestrians and cyclists to cross Downtown diagonally.

Currently the rail corridor is well used by pedestrians. Through landscaping and a designed pedestrian trail and appropriate crossings, the rail corridor can be made safe and attractive.

5.5.6　The tree canopy

The tree canopy should be expanded to include areas that are currently devoid of trees, particularly the Core Precinct and the riverfront. 5,000 + new trees should be planted along the riverfront and trees should be planted along the streets to create a 'green' visual linkage between the parks, the riverfront and the creek.

5.5.7　Streetscapes

Streetscapes shape the image of Downtown Moncton and link the area's open spaces, precincts and neighbourhoods. In the Vision, they are intended to serve as major pedestrian routes that are designed to unify the urban environment to create comfortable and appealing 'green' connections. Streetscapes should be enhanced with landscaping such as street trees, feature paving treatment, seating areas, distinctive signage and light standards.

5.5.8　The fine grain: smaller spaces

The fine grain smaller spaces are natural public spaces that are of a more intimate scale than neighbourhood parks. These smaller spaces, sometimes referred to as 'greens,' are passive spaces that serve as focus areas for predominantly residential neighbourhoods. The fine grain smaller spaces come in a variety of shapes and are generally bordered on all sides by roads. These spaces may contain pavilions, memorials, landscaped gateways and public art. They should be designed to provide a moment of respite (visual and physical).

It is imperative that these smaller green spaces engage pedestrians actively. They should be accessible and connected to doors and windows. It is not sufficient to adorn the side of a building with landscaping if it will remain unused and unavailable as an open space. Caution must be exercised to not employ smaller landscaped areas in such a way that they distance pedestrians from buildings, rather than bring them together.

5.5.9　Private Greens: front yards, rooftops and gardens

Private greens are open spaces within the private realm – front yards, rooftops, gardens and quasi-public open spaces. Similar to open spaces found within the public realm, private greens are also important in shaping the image of Downtown.

Landowners should be encouraged to enhance the appearance of their properties through landscaping and to maintain a certain level of upkeep so that both the physical and visual quality of the open spaces is high. This will enhance the public open space system by creating a seamless link between public and private open spaces.

The design of private greenspaces should fit with the overall urban structure of the City and should enhance the pedestrian experience. For example, a landscaped front yard on Main Street would detract from the character of the pedestrian realm and would not be consistent with the pedestrian environment of Main Street.

5.5.10 Winter Opportunities

The thriving of Downtown during all seasons will be dependent upon creating winter opportunities for use of public spaces.

Open space amenities should be designed and operated to remain accessible during the winter months (e.g. consideration of snow removal, storage and/or clearing). Locating indoor amenities close to outdoor amenity areas is one way to encourage use of certain amenities during the winter months, such as a canteen located beside an outdoor skating rink.

Assomption Boulevard, for example, should offer a direct access from the city to amenities located directly along the riverfront. A few pavilions with public uses on the riverfront (e.g. coffee shops, exhibits, performance venues) can offer refuge from inclement weather and help ensure the viable use of outdoor spaces year round.

5.5.11 Urban Trails

Urban trails serve pedestrians and cyclists and are similar to recreational trails with the exception that they link places of significance or theme. Urban trails provide tremendous opportunities to enhance the open space system through proper lighting, accessibility and clearly demarcated routes by using distinctive streetscaping and signage. Consider:

- **A culture/entertainment walking trail**
  Linking different cultural and entertainment destinations, such as theatres and museums, and places of significance, such as Bore Park, City Hall, the Capitol Theatre, the churches and the Aberdeen Culture Centre.

- **A heritage trail**
  Linking points of historical significance. From the east end to the west end, a series of heritage structures, landscapes and districts, and historical markers of significance should be linked by a trail that can be informative for residents and attractive to visitors.

- **Shopping routes**
  Shopping can be promoted through urban trails that create a clearly defined route connecting commercial and retail destinations throughout the Region and change through seasons and by themes. An urban shopping trail can be created connecting the Moncton Market with the market in Dieppe.

5.6 Expand the role for the arts

Art is an integral part of the history and culture of Moncton, and as such, art plays an important role in informing and contributing to the Development Vision for Downtown Moncton. Art in public spaces, architecture, landscape architecture, streetscaping and programming throughout Downtown should inspire and reflect the artistic culture of Moncton.

Art also plays an important role as a catalyst for tourism and local amenities.

A vision for the future of Downtown Moncton must also recognize that a creative approach, embodied in the enterprises of 'creative class industries' – graphic designers, architects, scientists, film producers, and artists to name but a few – are vital for the continued growth and success of Downtown.

Design competitions should be encouraged as a form of venue for artists that will also enhance the unique identity of Downtown.

5.6.1 Public Art

Public art can help establish and renew an identity that is unique to Moncton and elevates the profile and use of the public and private realm alike.

Examples of public art include:
- Contemplative sculptures for people to enjoy
- Playful sculptures and landscapes, especially for children
- Commemorative sculptures such as memorials and cenotaphs
- Landscaping features such as fountains, sun dials and seating areas
- Signifying elements such as gateways, signage and historic markers
- Ephemeral installations that promote festivals or special events
- 2-D graphics such as murals and pavement treatments

In some cases the idea of public art may also be expanded through an artistic approach to design, to include:
- Unique architectural features and details, such as a clock tower
- Significant landscaping, such as a flower garden
- Public infrastructure with enhanced designs, such as a bridge or a water tower
- Land formations and how they are used
- Any other surface visible from the public realm that is treated artistically

Public art should be installed at locations that are highly visible that will serve as accents to the precincts or neighbourhoods, as orienting devices for residents and visitors, or as focal points in public open spaces. In general, public art sites should consist of objects placed in or integrated into the urban environment.

5.6.2 Venues for Artists

To promote the arts in Downtown Moncton, venues for artists should be enhanced where they exist, and new opportunities should be sought. Civic spaces and plazas and other open spaces are a good location for such activities.
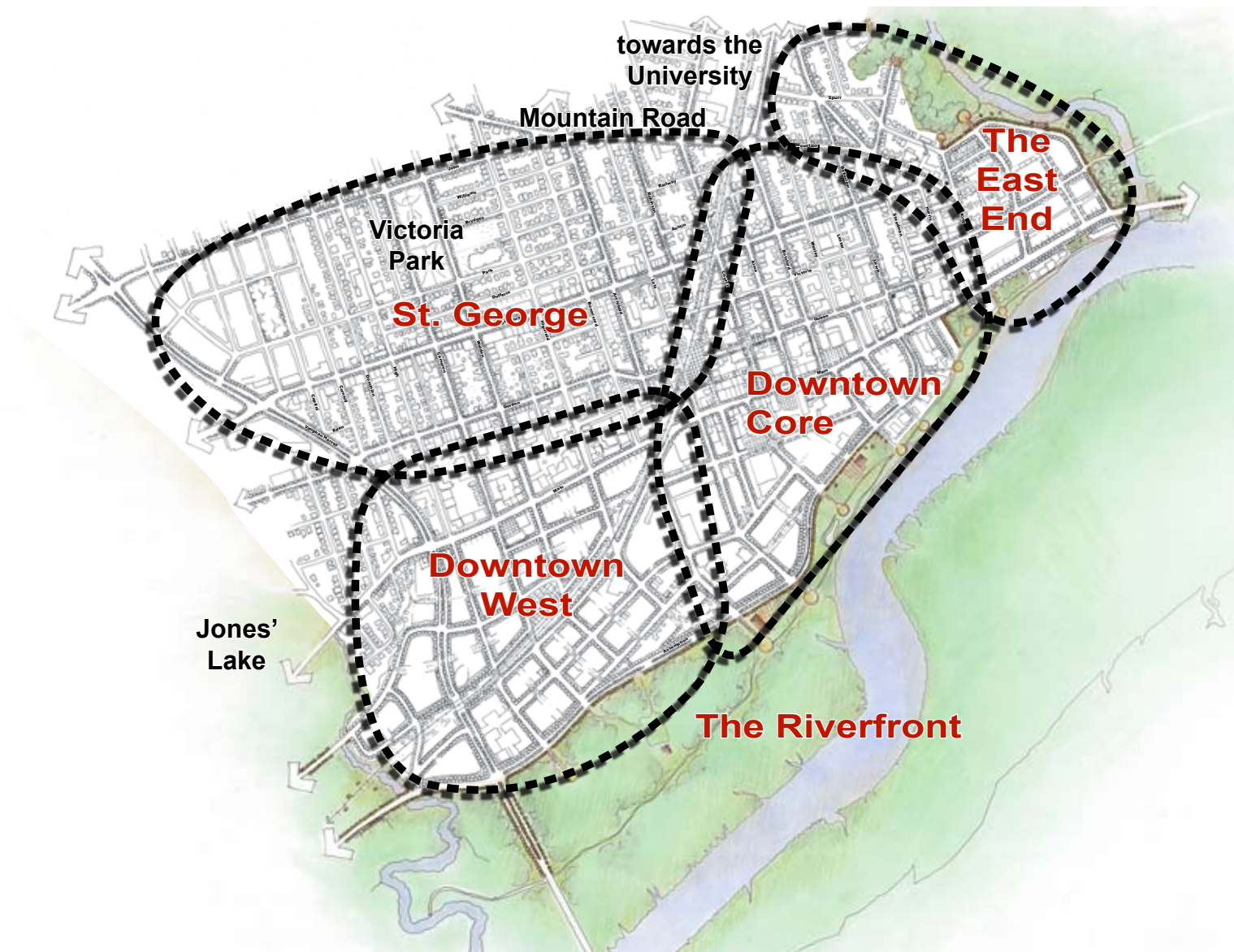
Studios, galleries, fringe performance venues will not only bring a critical mass of artists Downtown, but it will also promote tourism, both local and regional. Art begets interest and energy.

Downtown already hosts several indoor venues (Capitol Theatre, Aberdeen Culture Centre) and several outdoor venues (Bore Park, Victoria Park). As the park system evolves and new large areas are developed, maintaining a wealth of venues for artists to use and transform should continue to be of primary importance.

6

DOWNTOWN PRECINCTS

towards the
University

Mountain Road

The East End

Victoria
Park

St. George

Downtown
Core

Jones'
Lake

Downtown
West

The Riverfront

A distinct Precinct Plan, with guidelines and policies should be created, specifically catered for each identified precinct.

6. Downtown Precincts

Clearly identifying precincts and neighbourhoods will assist in the implementation of the Vision by breaking up larger areas into more manageable parts based on a sense of identity and recognizable features, uses, forms and functions. A distinct Precinct Plan, with guidelines and policies, should be created for each precinct to direct growth in keeping with area strengths and assets.

These Precinct Plan will also identify where where certain uses and capital investments should be directed, and will guide appropriate built form responses. The objective should be the maintenance of each area's distinct identity and yet collectively the Precincts and Neighbourhoods are to complement one another. Each plays a specific role within an integrated whole that is Downtown.

Sense of community and place will also be strengthened through the identification of Precincts and Neighbourhoods by reinforcing an area's identity through recognition of the built form, uses, heritage/history and the role it plays within the larger City. For example, the use of identifying design features (e.g. paving, signage, street furniture, sidewalk design) can unify the visual image of the different areas within Downtown. This strategy has been used successfully with the paving of Main Street.

The following pages describe the intended function and character of each of the Precincts and Neighbourhoods. To reinforce the distinction of each area, while contributing to Downtown's overall vitality and appeal, potential interventions, improvements and developments are identified and demonstrated where applicable. New developments in each one of these precincts should adhere to the overall vision and follow the guiding principles.

6.1 Riverfront

The Petitcodiac River is one of Greater Moncton's most distinguishing features and the riverfront is an unearthed jewel that has yet to realize its full potential. The Riverfront will function as Downtown's primary recreational amenity and public green space.

The Vision:

Open spaces are intimately connected to a network of parks, parkettes, and streetscapes, where every aspect of the public realm is exciting, welcoming, safe, environmentally sensitive and beautiful.

The open space system has enhanced and increased linkages to River and Hall's Creek (physical and visual). New connections are also established towards the west, through normalized intersections and the extension of Assomption Blvd. and Main Street.

Riverfront is a string of parks: a natural area, a central park, intimate areas, a necklace of parkettes, 5,000 trees, and historical sites.

The key objectives for the Riverfront are:
- To create a new heart for the city. The Riverfront should be a large green swath bordering a densely populated Downtown.
- Preserve open spaces along the riverfront for the enjoyment of residents and visitors – as a neighbourhood park and as a regional destination.
- To improve the quality of the open space and the health of the environment through extensive landscaping and tree planting.
- To provide a significant residential population through high quality housing choices north of Assomption Boulevard and a high quality public realm.
- To strengthen pedestrian east-west connections across Downtown.

- To improve the quality and appeal of the pedestrian environment by ensuring that amenities provide for convenience, comfort, safety, activity and visual interest on streets and open spaces at all times of the day, and during all seasons.
- To create a new neighbourhood by focusing new development on the north side of Assomption Boulevard, and allow no new development on the south side of Assomption Boulevard.

6.2 East End

The East End Neighbourhood is the historic focus for the City. This is where some of the early settlements were established. The East End functions as a cultural and historic focus for both Downtown Moncton and the Greater Moncton Area.

The Vision:

The East Side is coherent as a village, with modest residential areas, significant historic buildings (old hospital/convent, Marven's, Free Meeting House).

It truly reflects Moncton's cultural diversity and origins as a community built on the banks of the river.

This area is the Eastern Gateway to the City of Moncton reflecting the ship-building heritage (e.g. with a mass of mast-like forms set into the salt marshes on the banks of Hall's Creek).

The North side of Main Street from Lewis to Hall's Creek will become an extension of the character of downtown, with mixed use development.

Cafés, arts, retail, commercial and community organizations will abound at ground level; residences will be located above in 3-4 story buildings facing south and overlooking the river.

The key objectives for the East End are:
- To maintain and enhance buildings, landmarks and landscapes that contributes to the heritage character of the area.
- To ensure that infill and new development are compatible with the heritage character of the area and that they contribute to a vibrant and animated street life.
- To utilize Hall's Creek, creating an animated and appealing attraction that will redefine the image of Downtown.

- To enhance east-west connections in order to facilitate greater pedestrian movement, from Downtown to Hall's Creek and beyond to Dieppe.
- To reinforce the physical and visual linkages between the residential neighbourhoods and the open space system along Hall's Creek.
- To improve the quality and appeal of the pedestrian environment by ensuring that amenities provide for convenience, comfort, safety, activity and visual interest on streets and open spaces at all times of the day, and during all seasons.
- To create a compelling gateway and approach to Downtown from the east.

6.3 Core

The Core is Downtown's most recognizable precinct, functioning as the historic, commercial, business, civic and cultural heart for both the City of Moncton and the Greater Moncton Area.

The Vision:

The Core area south of Main Street to the Riverfront is a mixed use area that is welcoming to all ages, to live, work and play. It is characterized by a balanced mix of residential, retail, commercial and civic uses that have been created through the extension of the street grid.

A feature of this area are the three storey residential buildings with lovely shops at-grade that line the north side of Assomption Blvd, a grand pedestrian street lined with trees.

A civic use has been introduced at Assomption Blvd and Foundry Street.

The redeveloped core is an excellent place to shop and live: a regional destination.

The key objectives for the Core are:
- To maintain and enhance buildings and landmarks that contribute to the heritage character of the area.
- To ensure that infill and new development are compatible with the heritage character of the area and that they contribute to a vibrant and animated street life.
- To enhance the integration of uses: commercial, civic, residential, recreational, and tourism, as a way of maintaining an active population at all times of the day, every day of the week, and during all seasons.
- To utilize the riverfront, creating an animated and appealing attraction that will redefine the image of Downtown and help sustain a lively mixed-use environment.
- To enhance north-south and east-west connections in order to facilitate greater pedestrian movement across Downtown and towards the open space system (i.e. Riverfront, Hall's Creek and Jones' Lake).
- To reinforce the physical and visual linkages between neighbourhoods to the east, west and south towards the river.
- To improve the quality and appeal of the pedestrian environment by ensuring that amenities provide for convenience, comfort, safety, activity and visual interest on streets and open spaces at all times of the day, and during all seasons.
- To provide for adequate and accessible parking areas that are carefully designed so as not to detract form the heritage character and pedestrian orientation of the area.

6.4   Main Street

Although portions of Main Street are contained within the Core Area Precinct, Main Street as a whole (from Downtown West to the East End) has a unique identity and character all of its own.

Main Street will be the primary east-west commercial street in Downtown Moncton characterized by a mix of uses (retail, office, civic and residential). Although different portions of Main Street may have a unique character or identity, the pedestrian experience will be one that is seamless from east to west, drawing people along the entire street rather than just a portion of it. As such, Main Street will be both a local and regional destination.

The key objectives for Main Street are:
- To maintain and enhance buildings and landmarks that contributes to the heritage character of the area.
- To ensure that infill and new development are compatible with the heritage character of the area and that they contribute to a vibrant and animated street life.
- To develop a strong retail corridor at grade level, that is supported by a mix of uses above, commercial and residential.
- To enhance north-south and east-west connections in order to facilitate greater pedestrian movement.
- To reinforce the physical and visual linkages between Downtown neighbourhoods.
- To improve the quality and appeal of the pedestrian environment by ensuring that amenities provide for convenience, comfort, safety, activity and visual interest on streets and open spaces at all times of the day, and during all seasons.
- To provide adequate and accessible parking areas that are carefully designed so as not to detract from the heritage character and pedestrian orientation of the area.

6.4 Downtown West

The Vision:

Downtown West will be a thriving mixed use area consisting of a series of neighbourhoods centered on public spaces, cultural and civic buildings.

It will feature unearthing the existing creek and celebrating it as part of a thriving open space system.

Highfield Square will be redeveloped and integrated into neighbourhood blocks, thereby allowing for an extension of the street grid and connecting this newly developed area to Main St.

The existing train tracks will remain, but will be buffered with a berm that provides an appropriate separation between trains and pedestrians.

The Railway Lands can be divided into two districts: the Foundry, south of the tracks and Federal Square, north of the tracks.

6.4.1 Foundry

The Foundry is the area of Downtown West that extends south of the rail corridor. The proximity of the Foundry Precinct will create a natural extension of the Core Precinct, with similar functions. Foundry is deeply rooted in the transportation and industrial heritage of the City and as such, development will build upon and enhance this heritage, distinguishing this Precinct from the Core. Foundry will function as a primarily mixed-use residential area with civic and cultural amenities that will provide a transition from the commercial uses in the core with the open space amenities along the riverfront.

The key objectives for the Foundry Precinct are:
- To maintain and enhance buildings and landmarks that contributes to the heritage character of the area.
- To create a new neighbourhood, transforming the former

port and industrial uses into a lively mixed-use environment with an animated street life.
- To focus on the riverfront, supporting a well-used and well-connected attraction that will redefine the image of Downtown.
- To enhance north-south and east-west connections in order to facilitate greater pedestrian movement from Downtown towards the Riverfront.
- To reinforce the physical and visual linkages between neighbourhoods to the north, east, and west , towards the river to the south.
- To improve the quality and appeal of the pedestrian environment by ensuring that amenities provide for convenience, comfort, safety, activity and visual interest on streets and open spaces at all times of the day, and during all seasons.
- To develop an urban edge boundary along Assomption Boulevard, that is animated, unique, and a source of identity and pride for the entire City.
- To create a compelling gateway and approach to Downtown from the west.

6.4.2 Federal Square

Federal Square is the area of Downtown West that extends north of the rail corridor. Federal Square is the western gateway into Downtown. Similar to the Foundry Precinct, Federal Square will provide a transition from the commercial uses of Main Street to the residential neighbourhoods to the west. Federal Square will also act as a bridge between Jones Lake and the western neighbourhoods to the Core. Federal Square will continue to function as a hub for public transit, but will emerge as a mixed-use commercial/residential area with important institutional uses.

The key objectives for Federal Square are:
- To maintain and enhance buildings and landmarks that contributes to the heritage character of the area.
- To ensure that the area is developed as an entirely new

neighbourhood, with a new sense of urban identity, that remains highly integrated with the urban fabric of Downtown, with ample pedestrian connections, and a high quality public realm.

- To enhance north-south and east-west connections in order to facilitate greater pedestrian movement.
- To reinforce the physical and visual linkages between adjacent neighbourhoods.
- To improve the quality and appeal of the pedestrian environment by ensuring that amenities provide for convenience, comfort, safety, activity and visual interest on streets and open spaces at all times of the day, and during all seasons.
- To create a compelling gateway and approach to Downtown from the west.

6.5 St. George Street

St. George Street will be a new living choice for Moncton – it will serve as an urban residential main street comprising a series of neighbourhoods, created and defined through the infill and redevelopment of key sites.

The Vision:

> Each neighborhood contains a series of amenities essential to a "complete" community: a public park, with amenities for children such as washrooms and play equipment; libraries and other public, neighbourhood services; a bakery, a green grocer, and a place to repair your bike or a small theatre to watch a local play.

> The street is an active place where neighbours meet on widened sidewalks, and visitors enjoy the specialty boutiques in the shade of an ever increasing tree canopy. This character is repeated every five or six blocks. Residents walk three blocks south to the downtown core to work.

The key objectives for St. George Street are:
- To maintain and enhance buildings and landmarks that contributes to the heritage character of the area.
- To ensure that infill and new development are compatible with the heritage and neighbourhood character of the area and that they contribute to a vibrant and animated street life.
- To enhance north-south and east-west connections in order to facilitate greater pedestrian movement.
- To reinforce the physical and visual linkages between the surrounding neighbourhoods, primarily the residential areas to the north and south.
- To improve the quality and appeal of the pedestrian environment by ensuring that amenities provide for convenience, comfort, safety, activity and visual interest on streets and open spaces at all times of the day, and during all seasons.
- To provide for adequate and accessible parking areas that are carefully designed so as not to detract from the heritage character and pedestrian orientation of the area.

7

BUILT FORM GUIDELINES
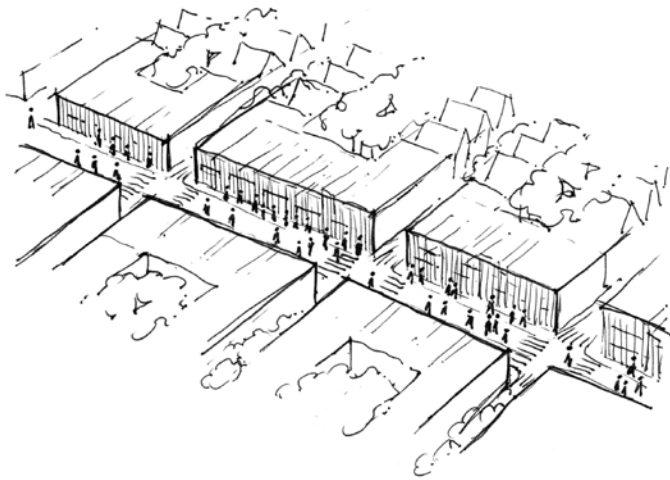
Encourage a compact street form
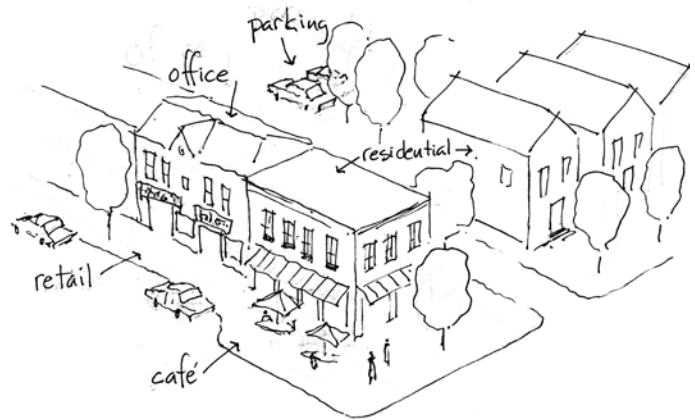
Concentrated, mixed-use development
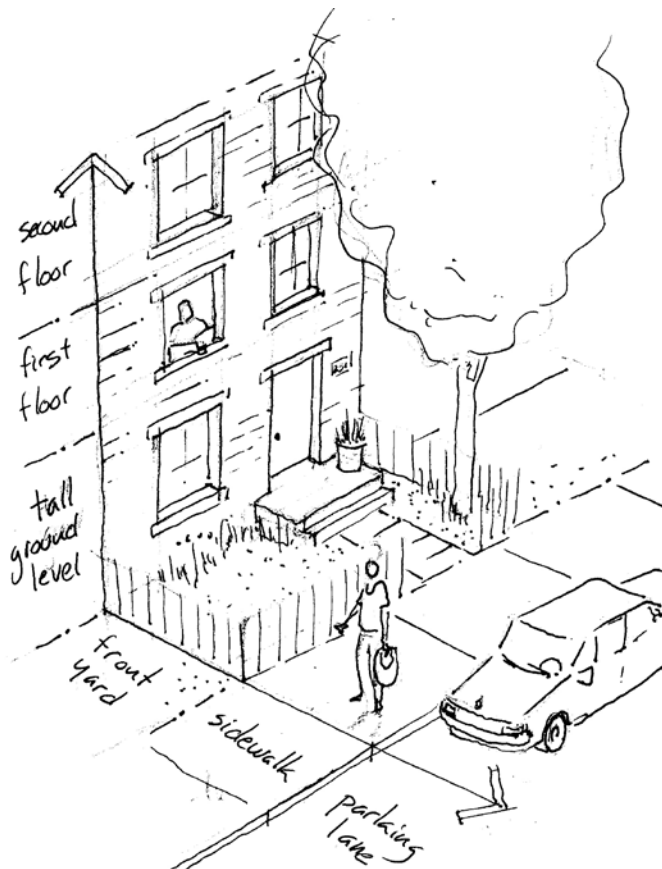
Create a strong link between interior/exterior spaces, sidewalk, and streets

7.1 Pedestrians First

Goal – To ensure that all areas within Downtown foster the best pedestrian environment possible.

By focusing on the quality and experience of the pedestrian environment it is possible to achieve many of the public realm objectives. If a street is welcoming and safe for a pedestrian it is likely that he/she will want to visit the area, do some shopping, bring a friend, and linger.

Encourage a compact street form

Compact streetscapes are such that people can easily walk between uses. Compact streets are characterized by minimum traffic lanes, a narrow but well used street section, compact relationship between the public and private zones, and an immediate connection between buildings, sidewalks, on street parking, the street, and all associated landscaping (e.g. front yards).

Pedestrians require a level of ease and comfort be maintained in order to continue walking. If a streetscape "breaks down", if there is an ugly part of the street, if the journey becomes unattractively long, people will stop walking. This is especially true in the winter. Therefore, it is critical for the success of a Main Street, and the appeal of any street, that pedestrians are constantly engaged and comfortable.

Compact streets heighten the pedestrian experience by encouraging the direct proximity of uses. They also minimize the extent of "no-man's-land" – portions of the right-of-way or private properties that are underutilized and often difficult to maintain.

A system of compact streets must be supported by a comprehensive network of interconnected streets. As the overall grid is extended each street assumes a small portion of traffic (vehicular and pedestrian). To the extent that a variety of options for movement are available, traffic and parking will be dispersed throughout Downtown.

Concentrated, mixed-use development

The downtown should be designed to support mixed and concentrated developments that offer a variety of uses within easily walkable areas (a ten minute radius), adjacent to transit nodes and major activity centers such as office clusters, commercial corridors, parks, and cultural and entertainment destinations. Also, encourage a mix of uses (including office, retail, housing or other uses) within a single project, whether it is a single building or the redevelopment of a large site.

Create a strong link between interior/exterior spaces, sidewalk, and streets

The strong spatial connections between the public right-of-way and the interior spaces of buildings enhance the successful use of both spaces. In general, the most successful urban open spaces are those with direct physical and visual access to adjacent streets. New buildings should make every effort to link interior spaces directly to the sidewalk, street and outdoor spaces through design as well as programming. Placement of entries, level changes, windows, doors and other elements should be used so as to clearly and seamlessly link interior and exterior public space.

- New buildings and public spaces should have direct access to adjacent streets. They should be open along the sidewalk and allow for multiple points of entry. They should also be visually permeable from the sidewalk, allowing passers by to see directly into the space.
- If there are substantial changes of grade between the sidewalk and the building there should be numerous points of entry along the sidewalk.
- Walls, fences and dense planting that visually secludes the interior space from the sidewalk should generally be avoided.

Safety

People will only walk if they feel safe: safe from harm, from crime, and from moving vehicles.

There are a variety of streetscaping elements that can help people feel safe, including pedestrian oriented lighting and maintaining clear and open sight-lines.

There are also important built form considerations to increase pedestrian safety. Dead-end streets tend to be less animated, and less used by pedestrians and therefore less safe. Buildings should be designed with many "eyes-on-the-street" through the presence of doors and windows facing pedestrian areas.

To minimize potential conflicts with vehicles, pedestrian routes should be direct and clearly demarcated. Where a main pedestrian route must cross a parking lot or driveway, provide a clearly distinguishable path – buffered by landscaping and other amenities, or use highly visible crosswalks made of material that provides strong contrast with the vehicular surface (e.g. concrete unit pavers) and the use of traffic calming devices to slow vehicles at crosswalks.
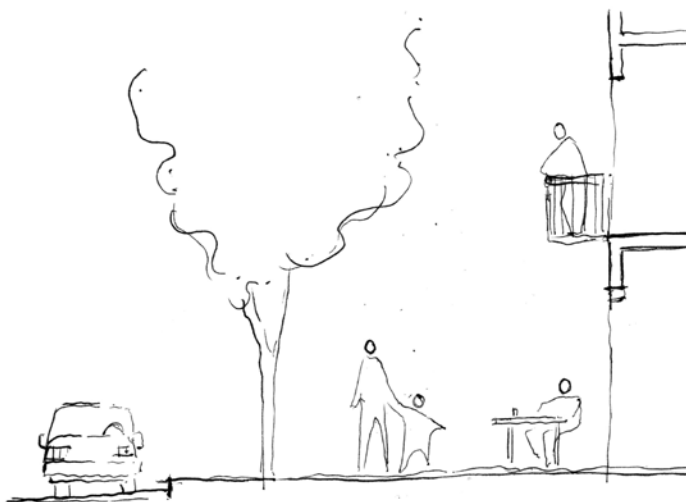
Maintain the street as a primary public space
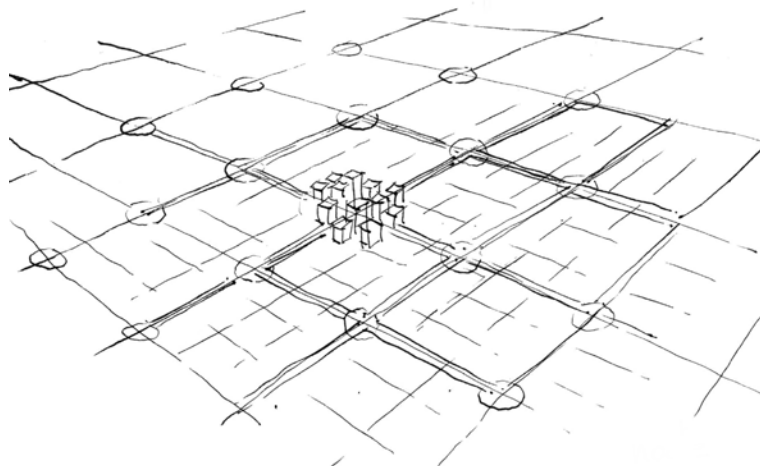
Maintain a connected grid of streets

7.2 Streets

Streets are integral components of the downtown experience. Streets should be designed as the main public spaces of the City and should be scaled to the pedestrian.

The urban pattern of Moncton's early settlement has a clear and efficient grid pattern. In contrast, the former rail lands and port lands present a discontinuous street pattern. As these former industrial areas are integrated to the city proper, new or improved streets should be modeled on the existing pattern of streets and blocks that exist within the historic core of the city.

Maintain a connected grid of streets

The Vision encourages the development of a network of interconnecting streets that disperse traffic while connecting and integrating neighbourhoods with the existing urban fabric of the greater Moncton area. Equally as important, is developing a network of sidewalks and bicycle lanes within the right-of-way that provide an attractive and safe mode of travel for cyclists and pedestrians.

A well connected and integrated grid of streets provides a number of benefits, including:
- Ensuring that vehicles and pedestrians travel a shorter distance. Which results in a) increasing the appeal of walking and cycling; and b) reducing the number of cars on the road.
- Distributing traffic through the grid, avoiding bottle-necks and grid-lock by providing different options for movement.
- Distributing parking through the grid. Both on-street parking and opportunities for mid-block parking lots.
- A grid system is better able to sustain a diversity of transportation modes, cars, pedestrians, transit, and cycling

because they are distributed throughout and not bundled into a single corridor.
- A greater number of street intersections create a safer and more efficient environment for pedestrians.

The grid of streets also extends beyond Downtown. Vehicular and pedestrian connections to other neighbourhoods, and areas adjacent to Downtown should be developed and strengthened (e.g. the West End, the University, Dieppe, Riverview) .

Maintain the street as a primary public space

The sidewalks remain the principal place for pedestrian movement and meaningful social interaction. Designs and uses should complement that function and give the highest priority to pedestrian uses on all streets.

New public open spaces should consist of either renovated or enhanced streets, or strategically selected places that are directly linked to the street system.

Generally, pedestrian ways should not be separated from streets and sidewalks, unless in large park systems such as the riverfront and Hall's Creek. Such conditions should be used sparingly and only in areas of high pedestrian traffic. In all cases, riverfront open space trails should be clearly and strongly linked to pedestrian oriented streets.

Streetscape improvements should be a constant goal, particularly in situations related to intensive activities such as major destinations, storefront retail, transit stops and parking locations.

Make streets two-way routes

Two way streets increase the ability for vehicles to move throughout Downtown and reach a destination with a minimum travel-distance. They also reduce the speed of traffic, making it safer for pedestrians. One way streets should be avoided within
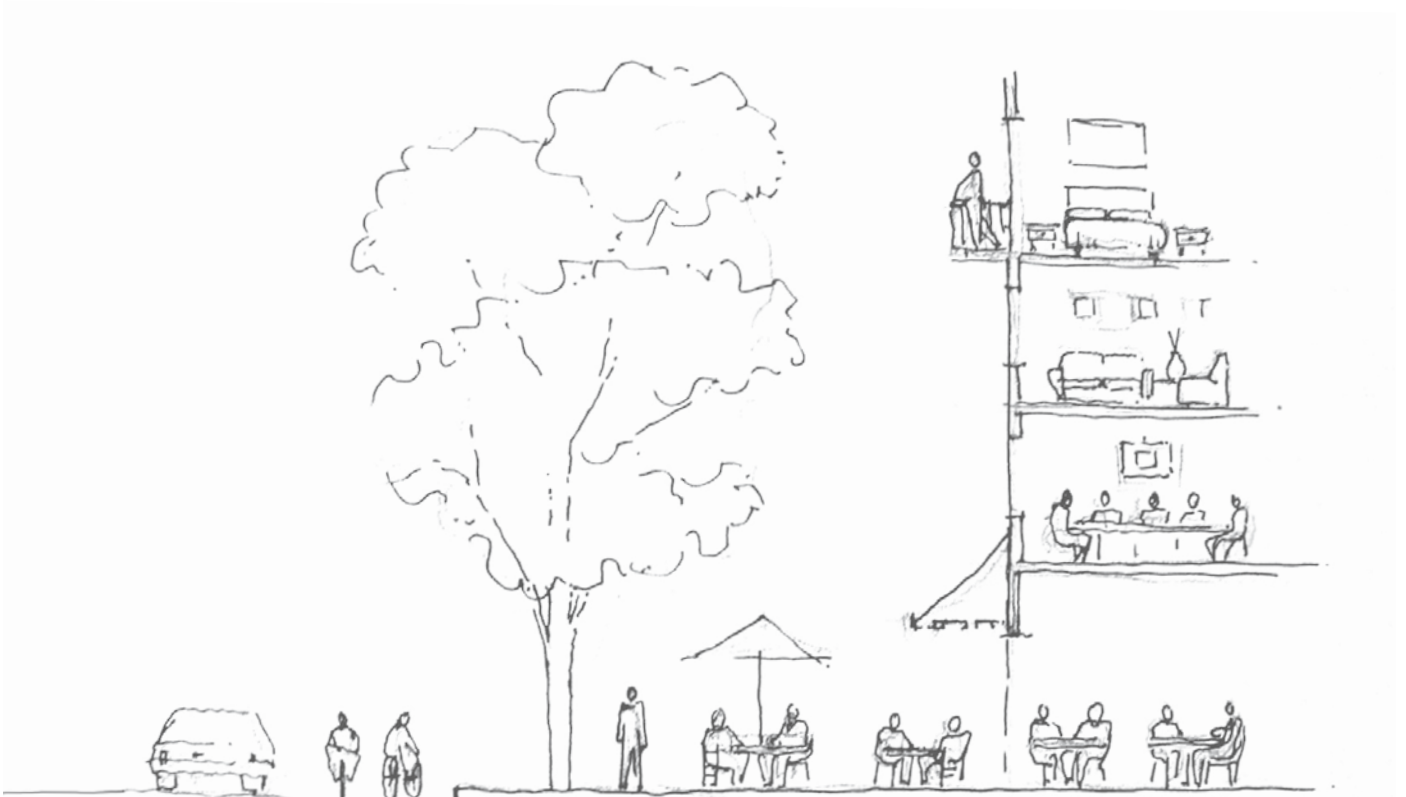
Provide an active edge
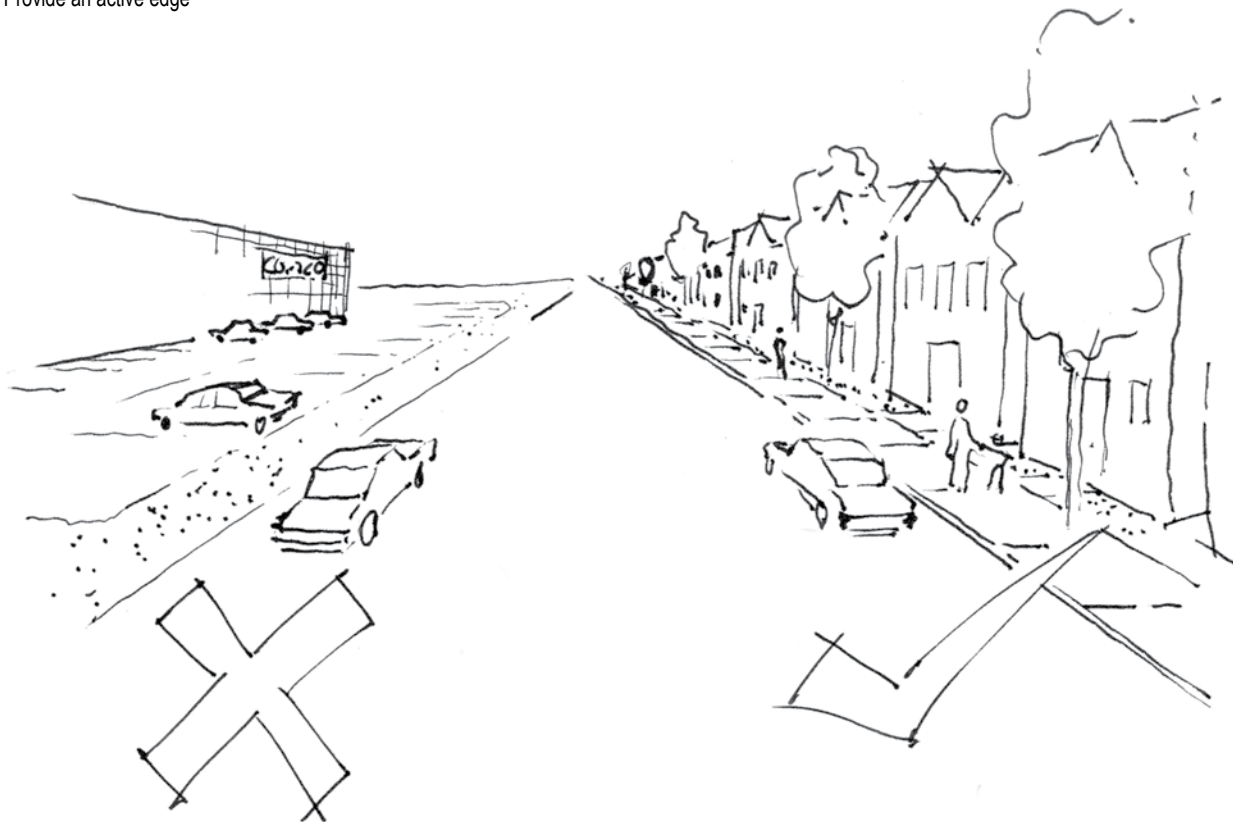
Avoid blank façades.

Downtown. Maintaining a system of two way streets will support a well connected transportation system that provides drivers with a variety of options and is able to disperse traffic throughout the system.

Provide an active edge

Pedestrian-oriented streets must have an active edge where the building frontages open out to the sidewalk through doors, windows and open views. This is applicable to all streets, including major thoroughfares.

The ground floors of buildings should be encouraged to contain public or semi-public uses such as retail or entertainment uses with direct entry from the street.

Lighting

Establish pedestrian scale street lights along street frontages where feasible. Pedestrian street lighting should illuminate the sidewalk at a level that is consistent with pedestrian activities rather than vehicular activity. Spacing should be standard but may vary to accommodate existing vehicular street lights or street trees. A custom streetlight fixture that combines both pedestrian and vehicular lighting could be considered on major streets. Pedestrian scale lighting may also be accomplished with fixtures that are mounted on buildings or located to accent architectural or landscape features. Such fixtures should be designed to enhance the overall architecture of the building, provide lighting for pedestrians and not damage historic materials.
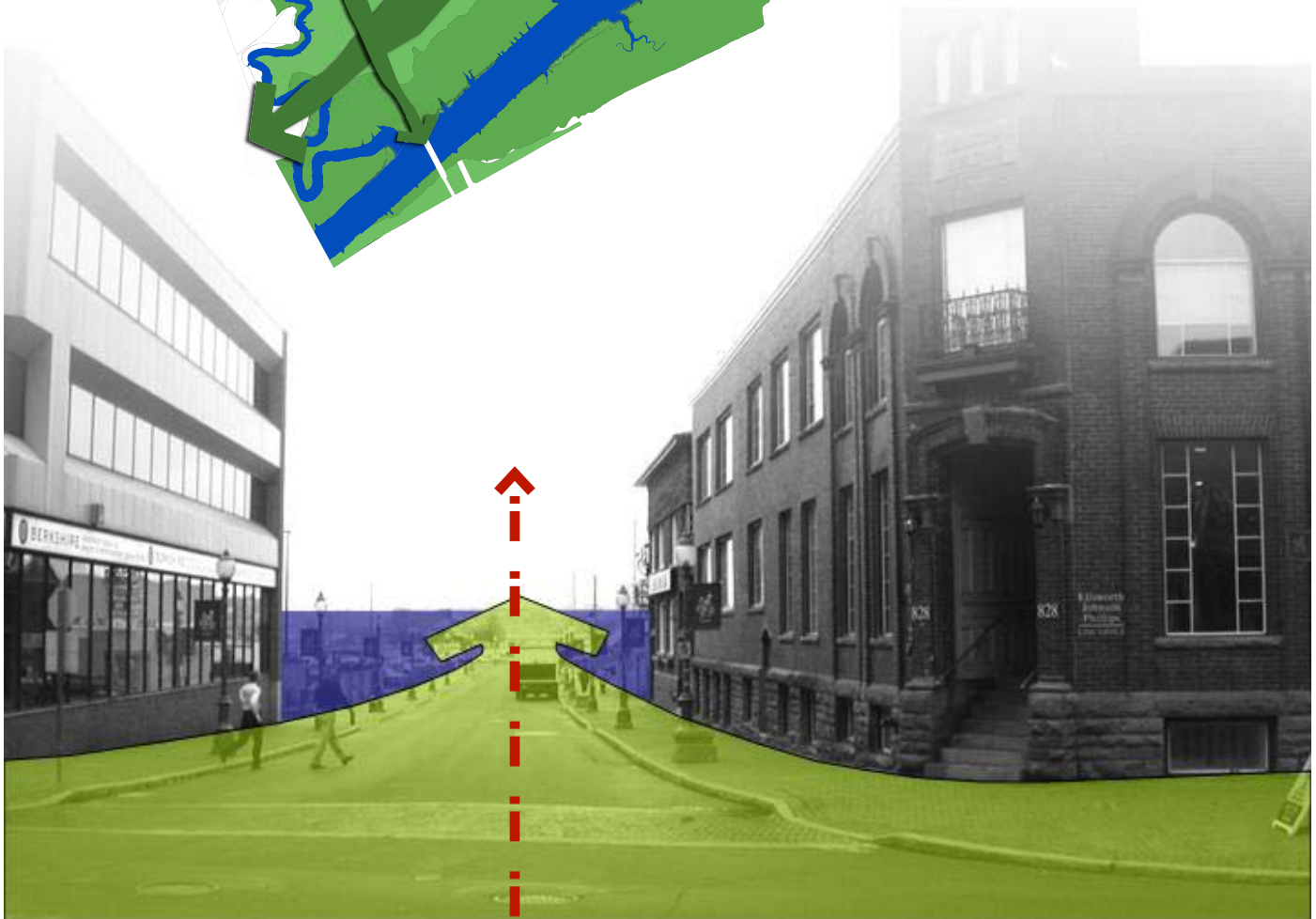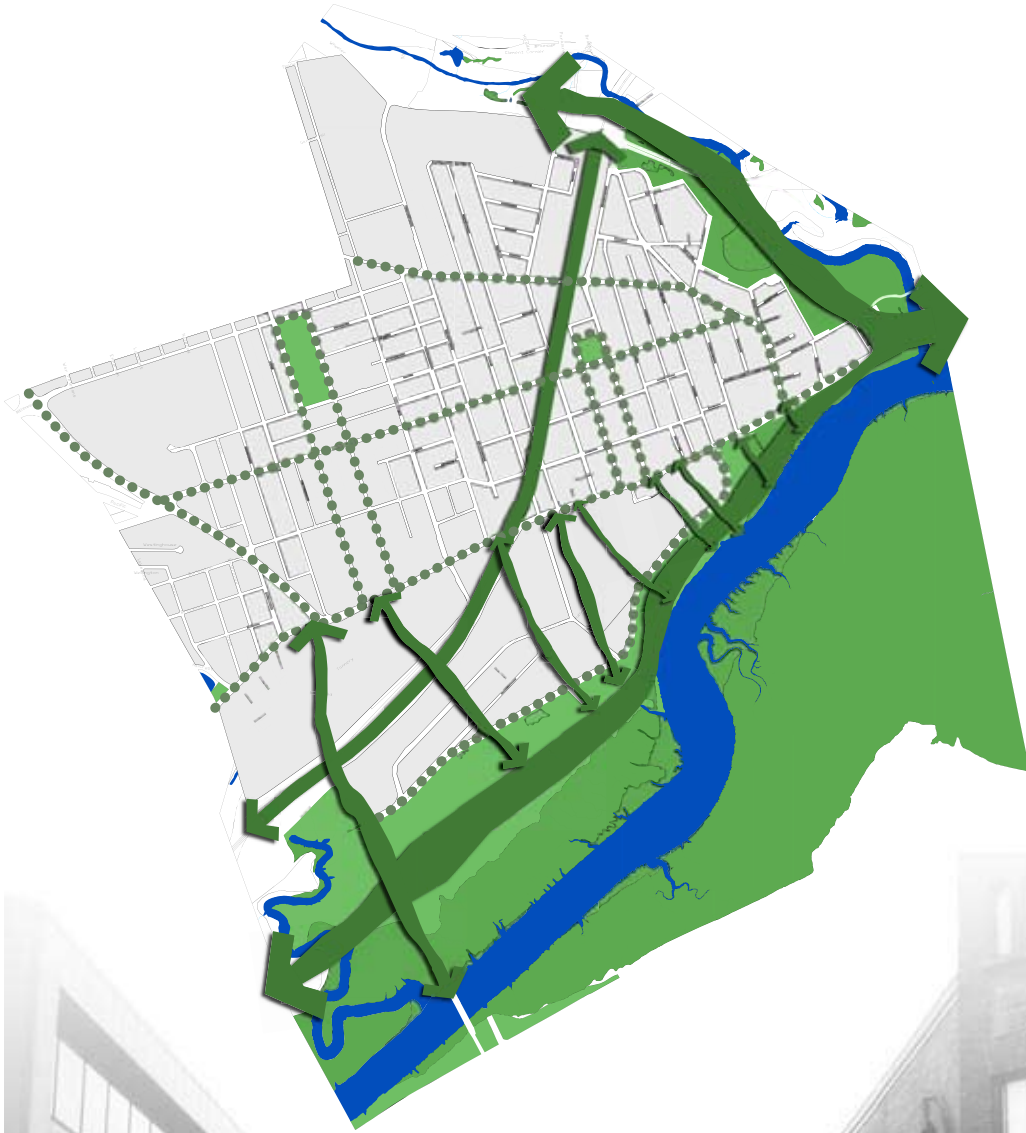
7.3 Physical connections and visual corridors

Goal – To protect and enhance physical and visual connections to important destinations, landmarks sites, and open spaces.

Previous sections have identified locations where important connections and visual corridors should be enhanced within Downtown. The built form has an important role to play in developing and denoting both physical and visual connections.

New and renovated buildings have the opportunity to both support existing connections, and create new ones. This is true for the entire Downtown, and especially significant at the smaller scale of individual buildings, as they are able to enhance access (physical and visual) to landmark sites and important public spaces.

Turn to face the water

Historically, buildings in Moncton were developed to face away from the Petitcodiac River and Hall's Creek. As these have shifted from satisfying an industrial use to providing a recreational amenity and public landscape, they must acknowledge the fact by facing the water. The face of the building can be defined through architectural quality and features such as a distinct building entrance, ample use of doors and windows, and a direct relationship between the building edge and the pedestrian realm.

Enhance connections between uses and users

The activities of adjacent buildings and/or open spaces should be connected to each other where appropriate. For example, areas used by large groups of people (e.g. a theatre) should be connected to open spaces where people gather to sit, eat, or watch other people.

As buildings are renovated or developed, consideration must be given to locating sympathetic public uses in proximity to each other and to areas of pedestrian circulation. A restaurant with an outdoor component, for example, can be well located directly onto Main Street or Assomption Boulevard and develop synergies with an active pedestrian environment.

Support the system of visual corridors

Individual buildings and landscapes have an important role to play by: a) not obstructing views; b) framing views; and c) signalling where views are present.

For example: the views to the riverfront are of primary importance. Buildings that are located along a street that lead towards the water should add-up to create a clear frame that contains the river view. Corner buildings on Main Street may have an added feature (such as an inset, or cut-off, corner) to provide a signal for pedestrians that something important is happening, just around the corner).

## Promote a high level of transparency at the street level

The level of transparency is the degree to which pedestrians are able to see through, and into, the building mass. A streetscape with lots of windows will have a high degree of transparency. Conversely, a black wall will minimize the views and opportunities for visual interaction by pedestrians.

A high degree of transparency, especially at the street level creates a welcoming, stimulating and safer environment to walk by.

Guidelines for transparency are often expressed as a development standard that defines a requirement for clear or lightly tinted glass in terms of a percentage of the façade area. Residential areas may have a 50% requirement, Main Street may be closer to 80-100% transparency at street level.
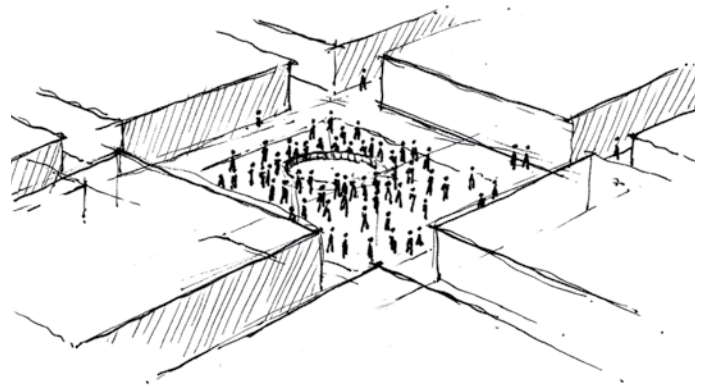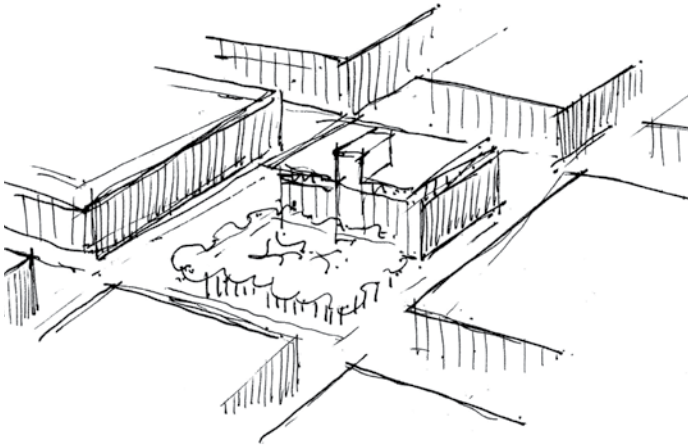
Recognize sites of public significance as urban landmarks
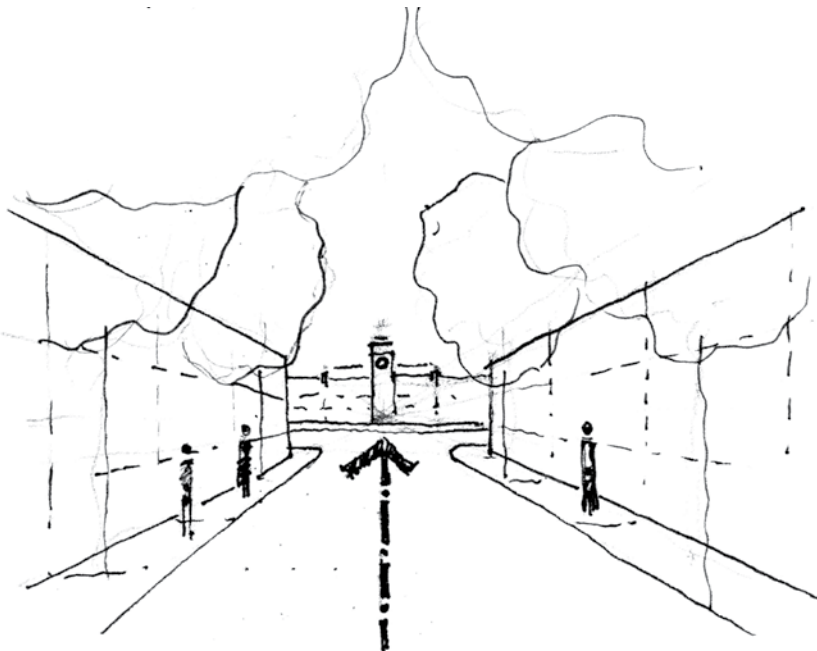
Recognize and design appropriate visual termini

7.4 Landmarks and Visual Termini

Goal – To protect and enhance landmark sites within Downtown.

Recognize sites of public significance as urban landmarks

Certain elements within the urban fabric stand out from the background by virtue of height, size, design or some other aspect that make it culturally significant. These can be buildings, landscapes, topographies, or even places of historic and cultural relevance. Through prominence they achieve a landmark condition.

Certain uses within Downtown stand out by the number of visitors or their prominence during different times of the day and/or season. City Hall, the riverfront park, the Capitol Theatre, a retail strip and the Market, are all examples of important destination sites within Downtown.

All destination sites of public significance should be recognizable as urban landmarks through distinct design features. This will connote their importance and help people orient themselves as they move from one destination to another.

Recognize and design appropriate visual termini

A visual terminus is the end point of a view line, accentuated through design elements often adding/subtracting from the building mass and/or landscaping.

Placing significant buildings and open space features at the end of visual corridors, and designing a visual termini component to such sites and buildings is a good way of acknowledging their importance.

Main Street presents a discontinuity in the street grid, which results in several streets terminating directly onto Main Street (both from the south and the north). The opportunity is to design the buildings that terminate each street (and view) in a way that recognizes the prominence of both the view corridor and the main street environment that runs perpendicular to these views.

Provide visual clues and signifiers to assist with orientation

Elements of prominence (such as the Aliant tower, formerly the NB Tel tower) function as orienting devices. At a smaller scale elements such as corner treatments, street furnishings, and the choice of materials can help provide pedestrians with the visual clues that demarcate significant destinations or areas of special identity. The paving of sidewalks on Main Street, for example, clearly defines an area and assists visitor's sense of orientation.

7.5 Heritage sites, landscapes and districts

Goal – To protect and nurture significant built heritage resources and significant cultural heritage landscapes.

There are two broad scales at which heritage resources must be considered: the smaller scale of individual buildings or elements and the larger scale of entire landscapes or districts (a previous section has identified areas that should be considered for Heritage Area Studies).

Any intervention Downtown, be it the renovation of an existing building or the development of an entirely new area, must always ask how it contributes to the overall sense of Moncton's cultural heritage and how it contributes to the heritage stock that forms part of its context. The purpose is to:

- Identify, maintain and enhance the unique identity and heritage character of Moncton.
- Preserve significant landscapes, buildings and building features, and ensure new designs contribute to their heritage character.
- Ensure that changes to valued heritage resources enhance the heritage character of the whole area.
- Encourage the redevelopment and revitalization of underutilized sites to enhance the heritage character.

It is important to note that the intent of focusing on heritage is not to "freeze" any building or streetscape in time, but rather to identify the valued attributes that should be carried forward as the city grows and evolves. To this effect, the adaptive use of the valuable historic building stock is considered an effective sustainable practice and is encouraged.

Consider the street wall
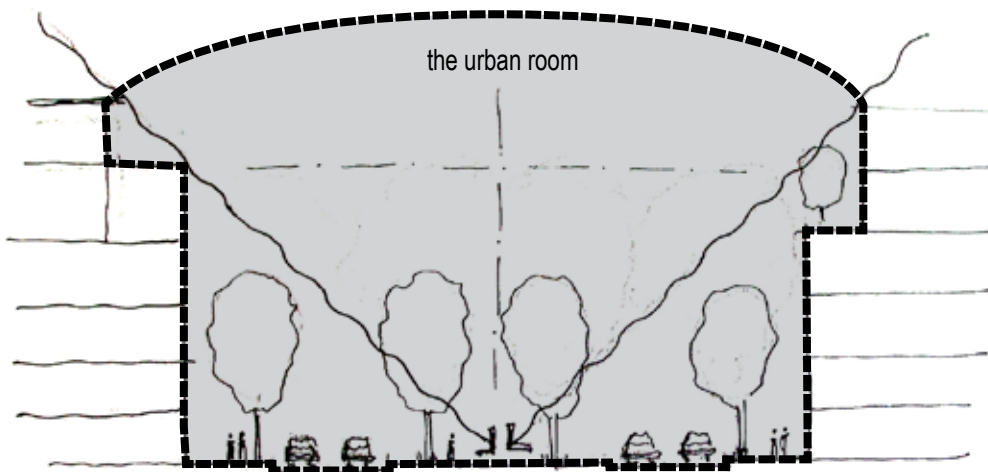
the urban room

Consider the urban room

7.6 Building Placement and Frontages

Goal – To ensure that new and renovated buildings are considered as part of their greater context and support a quality urban environment.

The Downtown is an example where the whole is greater than the sum of its parts. Each individual building, each part of a streetscape, and each park and open space, comes together to form a complete urban environment. In this sense, the presence of a single building – positive or negative – will ripple out to influence its entire context. One low quality development will reduce the flow of pedestrians along a main street, as much as an excellent building can become a regional destination.

Each building should be designed to form part of the larger area within which it is located (i.e. sensitive to the surrounding urban, built and natural conditions).

Consider the street wall

Buildings serve to define streets (and the public realm) spatially. Proper spatial definition is achieved with buildings or other architectural and landscaping elements that make up the street edges. A condition of alignment occurs when the façades of buildings add-up to delineate the public space, as walls form a room (i.e. the "street wall").

The cumulative effect of buildings lining the street through a common setback (distance of the main elevation from the public right-of-way), provides a clarity in the definition to the public realm and builds a collective character for an area. This is especially important in urban corridors such as Main Street, where buildings are consistently built to the property line (zero setback). As the Main Street experience is extended east and west, it is vital that new buildings maintain the integrity of the street wall character.

Consider the urban room

The relationship and ratio of building height to street width (the street section) defines the relative scale of the street. This condition is often characterized as the "urban room". A narrow street with 2 to 3 storey buildings has a very different character than a 10 lane boulevard with 10 storey buildings – the urban room in each case offers pedestrians a different experience.

As a general principle the ratio of height to width should be 1:1. Vaughan Harvey Boulevard, for example, will warrant taller buildings than smaller streets. The height of buildings however, must also be also be considered in relation to sunlight penetration, shading on the public realm, views, and the overall character of the area.

Public spaces for public uses

Major new public facilities such as theatres, community centres, and new municipal buildings, and the public areas of all buildings such as retail uses, should have clearly defined and significant public open spaces associated with their principal entrance and facades. These public spaces (interior and exterior) should be designed in such a manner that they are amenable and flexible for small, informal use as well as for large assemblies, during all seasons. In the small scale, the entry hall of a building should be linked to both a porch and an animated sidewalk. At a larger scale, a theatre should be linked to an outdoor place of gathering such as a plaza, a series of restaurants and cafes, or a main street.

Consider the building orientation

All buildings must have their primary orientation facing public streets, parks and greenways.

A clear example of this imperative is Assomption Boulevard. As individual buildings are developed on the north side of the street with facades animated by doors, windows, and the presence of people walking in and out, this urban edge will become one of the most welcoming and distinct streets in Moncton. If a building were
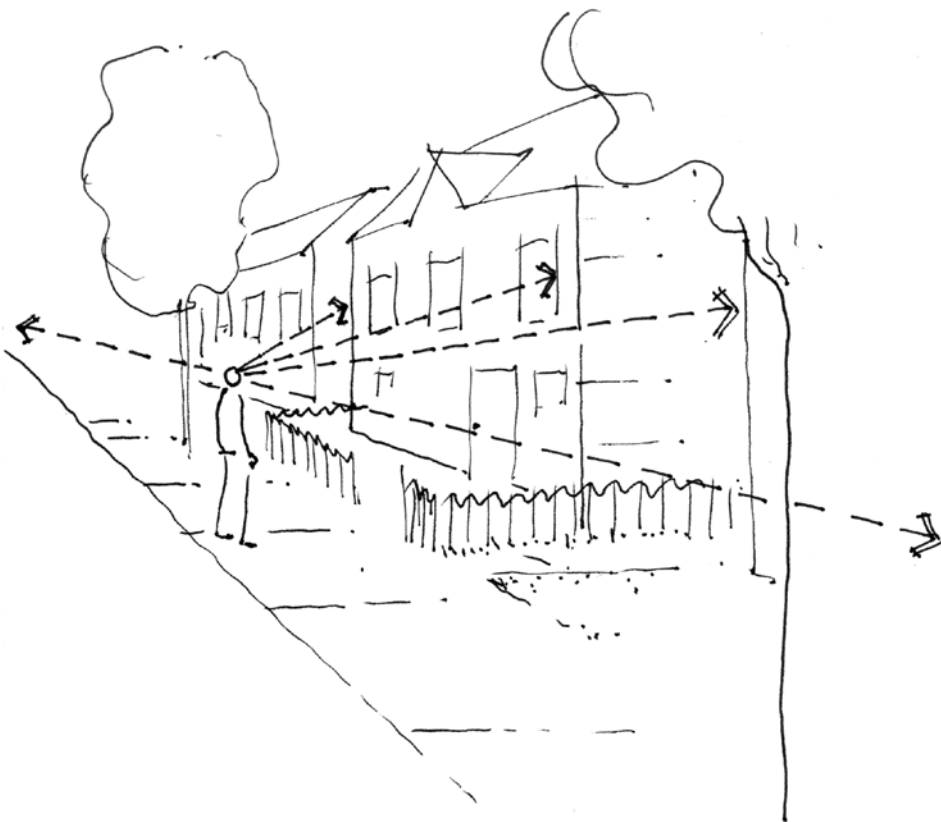
Ground floor facade facing a public street should be transparent, designed to allow pedestrians to view activities inside the buildings.

to turn its back to the street (as the former industrial buildings have) it quickly compromises the pedestrian appeal.

Additionally, buildings should be oriented to maximize views and natural lighting and ventilation.

Ground floor treatment - place activity at the street level

It is impossible to overemphasize the importance of an animated and transparent ground floor to create a welcoming streetscape. Ground floor facade facing a public street should be transparent, designed to allow pedestrians to view activities inside the buildings. Retail activities within buildings should be oriented towards the street and have direct access from sidewalks through storefront entries. Internal, vertically organized retail malls are discouraged. Ground floor storefront restaurants are strongly encouraged to have multiple doors, operable storefront windows and sidewalk cafes to increase the connection between the interior and exterior environments.

If a building is long or large, more than one entrance may be needed on the front façade, or entrances may be needed on a number of building sides. Uses on the upper floor must also be accessed through doorways with primary entrances on the main street. Entrance from parking facilities should be considered as secondary.

Consider taking the "indoors" outdoors by spilling interior space (e.g. dining areas, small merchandise displays) onto walkways and plazas. Small sidewalk displays help bring the indoors outside and increase pedestrian interest.

Animate the elevation

Building facades should be varied and articulated to provide visual interest to pedestrians. Numerous windows and building entries are encouraged. Streets with monotonous and unarticulated facades are hostile to pedestrian activity.

Appendages such as porches, balconies and bay windows may also be encouraged in certain areas to promote visual interest.

Screen service areas

Utilities and services, while essential to urban development should be screened or otherwise hidden from the view of the pedestrian.
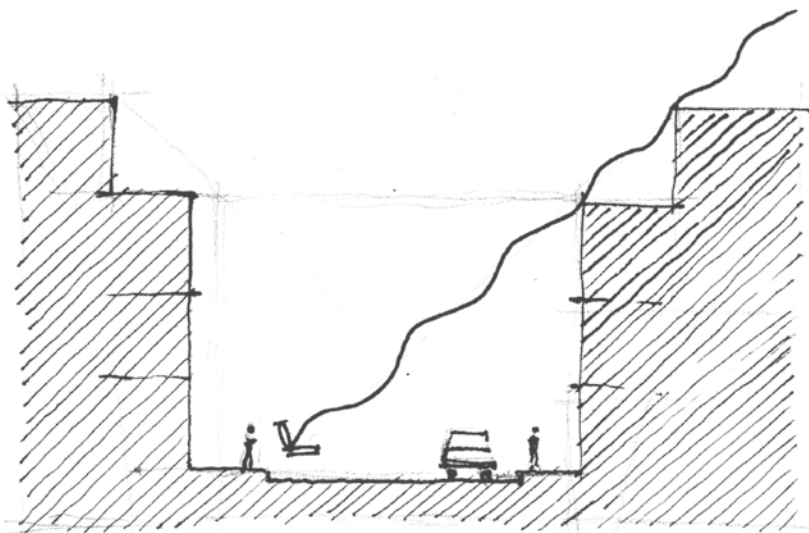
Consider sunlight and shadows

7.7 Height, Mass and Scale

Goal – To ensure that the building envelope of new and renovated buildings make a positive contribution to adjacent properties and public realm.

Two parallel imperatives face development Downtown:
1. to increase density, as a sustainable means of using urban resources; and
2. to enhance the quality and appeal of the urban environment.

Both these objectives are intertwined. Moncton has an extraordinary opportunity to absorb a significant amount of growth within Downtown, through the infill and development of entirely new areas such as the north side of Assomption Boulevard and the former railway lands. Developing underutilized sites, large and small, must be seized as an opportunity to enhance both the use of urban spaces and their appeal.

Developing vacant properties, brownfields and surface parking lots, is a sound sustainable practice. The resulting increased density enhances the use of public amenities such as parks, theatres, and retail, making them more successful and more worthy of public (and/or private) investment. It also implies that Moncton will grow inward, rather than spread out into greenfields. A critical density supports a walkable environment (e.g. there are enough people to make corner stores viable).

The opportunity will be lost if new developments do not enhance the quality and livability of Downtown.

Consider the human scale

The "human scale" is the impression of a building or architectural element when seen in relation to its users – the size and proportion of parts of a building or its details, that relates in a positive way to the visual and physical experience of a pedestrian. For example, a staircase or a doorway are inviting indicators to pedestrians, as they provide something they can relate to. A small door with a porch will indicate that this is probably a residential unit. A large door with a grand staircase denotes public or civic buildings. By contrast, a blank façade makes it difficult for pedestrians to know the scale of uses or even to judge distance.

Encourage a fine grain

Fine grain refers to an architectural pattern of buildings or design elements that is small and frequent, thereby creating a dynamic and animated urban environment for the pedestrian.

For human scale and visual interest the horizontal and vertical mass should be broken down into a hierarchy of volumes. Do not create large monolithic structures. Within each volume or bay there should be an orderly placement of windows and doors.

Consider sunlight and shadows

One of the most measurable ways in which a building impacts its context is through the shadow it casts. New buildings should be able to maximize the development potential, without causing undue shadow impacts on neighbouring structures and uses.

Certain uses Downtown necessitate a high degree of sunlight penetration, such as parks, schools, and public amenities. The extent to which warm sun penetrates these areas during portions of the day will increase their use, especially during the colder seasons.

Similarly, consideration must also be given to the skyview, that is, the extent of blue sky which is visible from any given point.

A design technique, by which buildings can increase height without increasing their shadow (or restricting the skyview) is by building within an angular plane. The angular plane can be defined through design guidelines and requires buildings to stepback or "terrace" respecting a certain angle (e.g. a 45 degree angular plane).
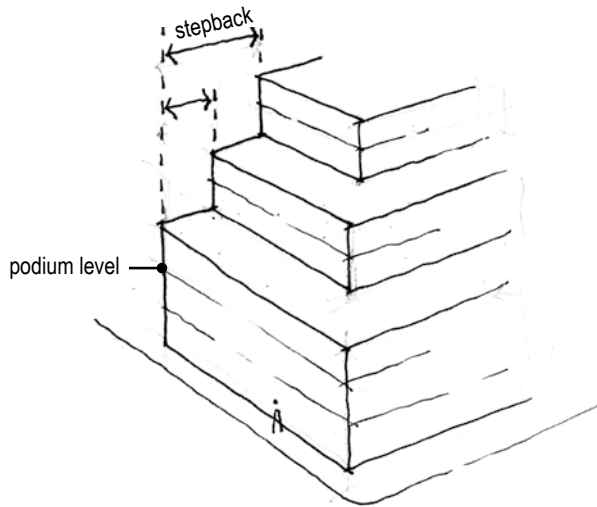
Use height strategically
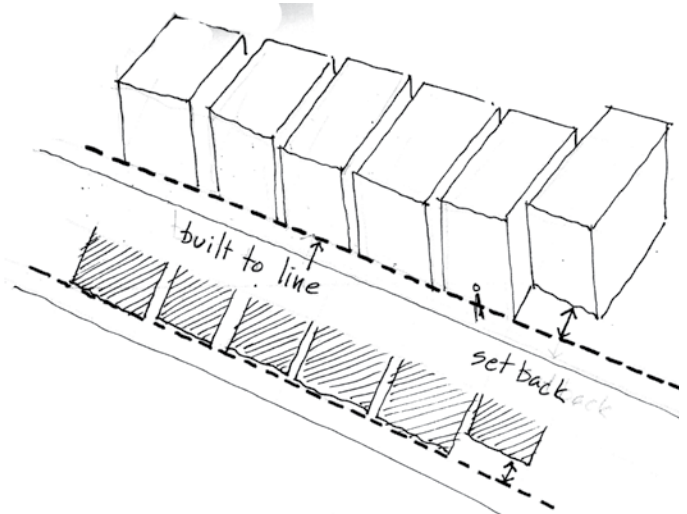
The setback of buildings should be consistent with adjacent structures to provide a continuous street wall. In the illustration above, the setback of a single building on a street where all other buildings create a continuous street wall, is not desirable.
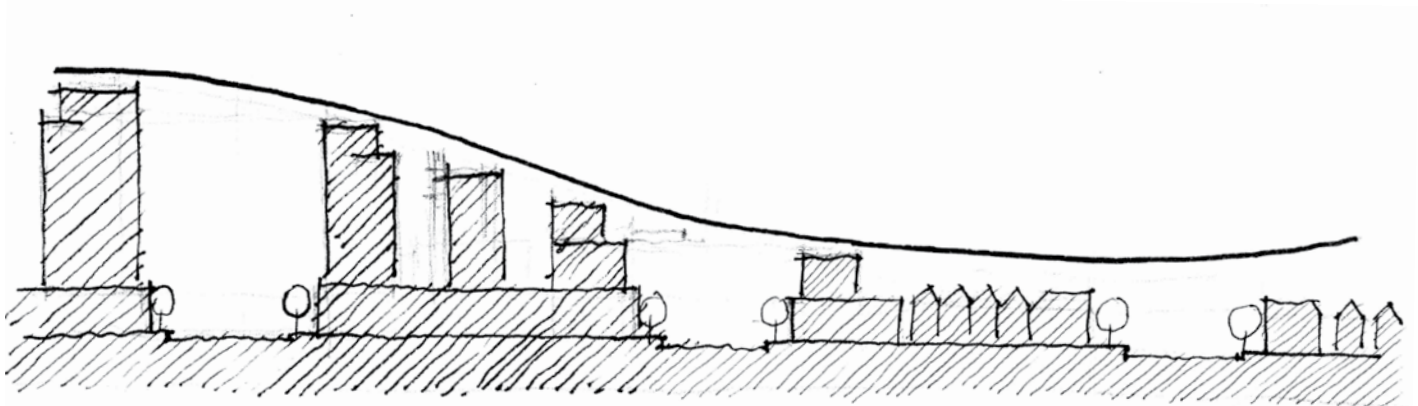
Provide a transition in heights

Use height strategically

A podium
At street level, pedestrians will typically interact most with the first 3 to 4 floors of a building. Developing a podium – the continuity of the building base along the entire length of the property and the block – is a good way of establishing a well defined and animated streetscape.

Stepbacks
Above the podium, buildings can and should stepback. Terracing higher portions of a building (the body) to have a lesser impact on the street will increase skyviews and sunlight on the pedestrian realm.

The point tower
When significant height is considered (10+ stories) the shape of a slender tower is desirable. A point tower with a gross floorplate of a maximum 600 meters allows open views and sunlight to be maintained. As well, the shadow of a point tower moves quicker than the shadow of a building with a larger floorplate.

Provide a transition in heights

New and renovated buildings should respect the scale of adjacent buildings by providing a proper transition – the tapering of building heights as a way of achieving compatibility of built forms and mitigating impacts (views, sunlight, etc.) of shifts from areas of one character (i.e. low-rise) to another (i.e. high rise).
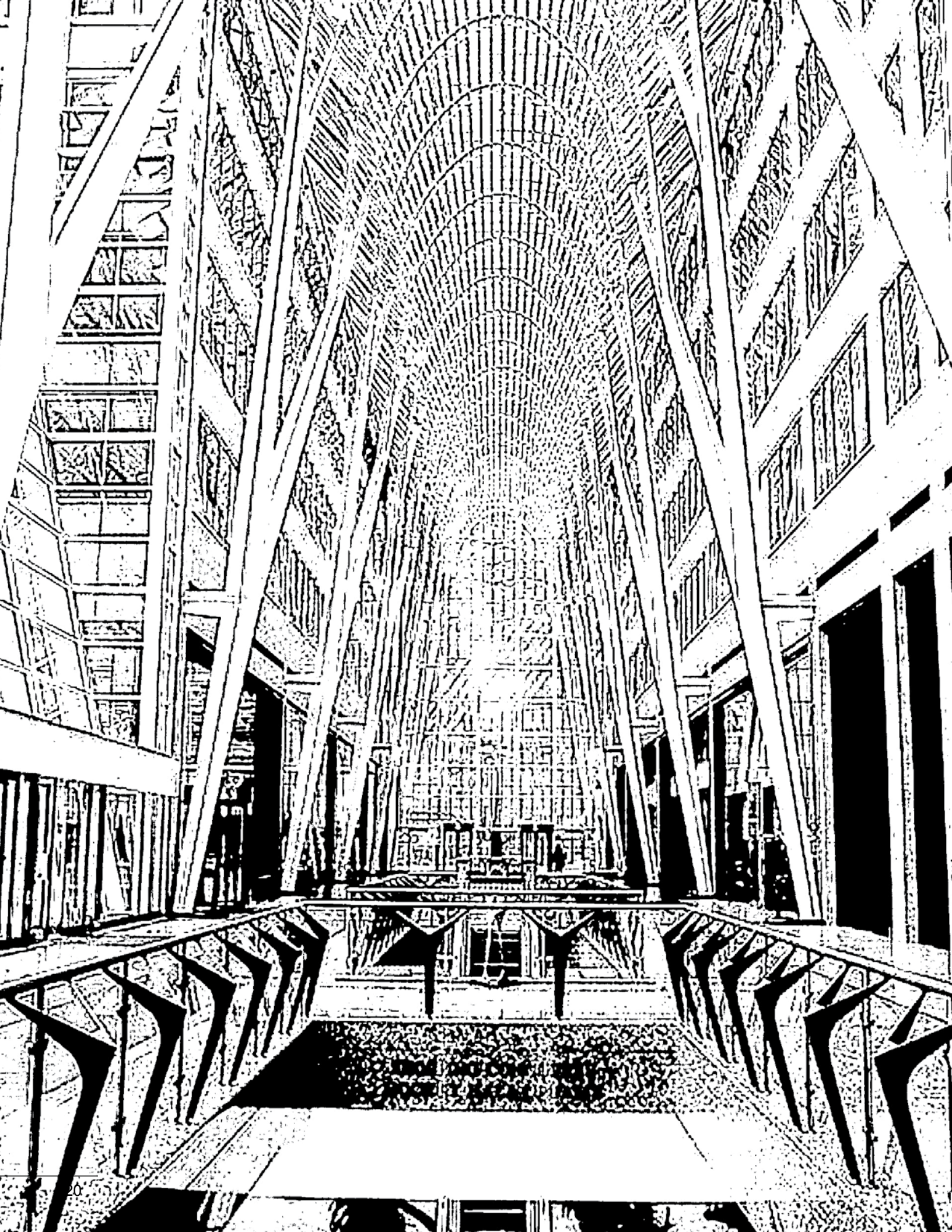
7.8 Public Art

Goal – To promote the use of art and an artistic approach in the public realm.

Public art can enrich the downtown experience, enhance its public image, and add beauty. Art in all its forms – graphic, sculpture, architecture, landscape architecture – should be a primary tool in the representation of an area's identity and character.

Consider:
- The context and character of the area surrounding the art site and the relationship of public art to its proposed site and its visual impact.
- The ability of public art to enhance the downtown experience such as bringing people together, inviting public interaction, creating moments of visual or intellectual interest, and enhancing the area's identity and beauty.
- The placement of public art to terminate a vista or serve as the focal point.
- The human or monumental scale of artwork located along public streets.
- The artwork's symbolic and aesthetic qualities.
- The durability of materials, maintenance and upkeep in public settings.

When related to a specific landmark building, consider that:
- The artwork should be integrated and subordinate to the overall building.
- The artwork should not obscure building elements or details. For example, a mural should not cover windows.

Ultimately, a creative and artistic approach should permeate all aspects of design. For example, even public infrastructure improvements, such as the 'subway' (at the junction of Main Street and the train tracks) should be used to enhance the identity of the public realm. In this case, extensive landscaping already exists in the northwest corner. Additionally, the impulse exists to use art as a way of giving the subway an attractive presence by: painting it pink!, creating a mural, or even by holding an art competition.

7.9 Environmental Design

Goal – To satisfy human activity using resources efficiently and effectively, and with minimum stress on natural systems.

All interventions Downtown should be conscious of, and responsive to, the ecology of the site and of existing natural systems. The adoption of this Vision is an opportunity to actively promote environmental sensitivity and sustainable practices in the construction and long-term use of all developments. Once the momentum is established, and the understanding of natural systems and environmental technology evolves, aspects of "sustainable" building design and operation will continue to be redefined.

Consider:
- Promoting and/or adopting performance Objectives (e.g. LEED, etc.)
- Strive to reduce overall quantities of energy used and to use sustainable energy sources as much as is technologically possible.
- Approach development and operation with the aim of using land, material and water resources in the most efficient and effective manner.
- Ensure that indoor environments are healthy and comfortable.
- Engage in comprehensive evaluations of design alternatives and operation strategies that consider the long-term.
- Building design should make environmental strategies explicit so as to communicate an emerging emphasis for these considerations
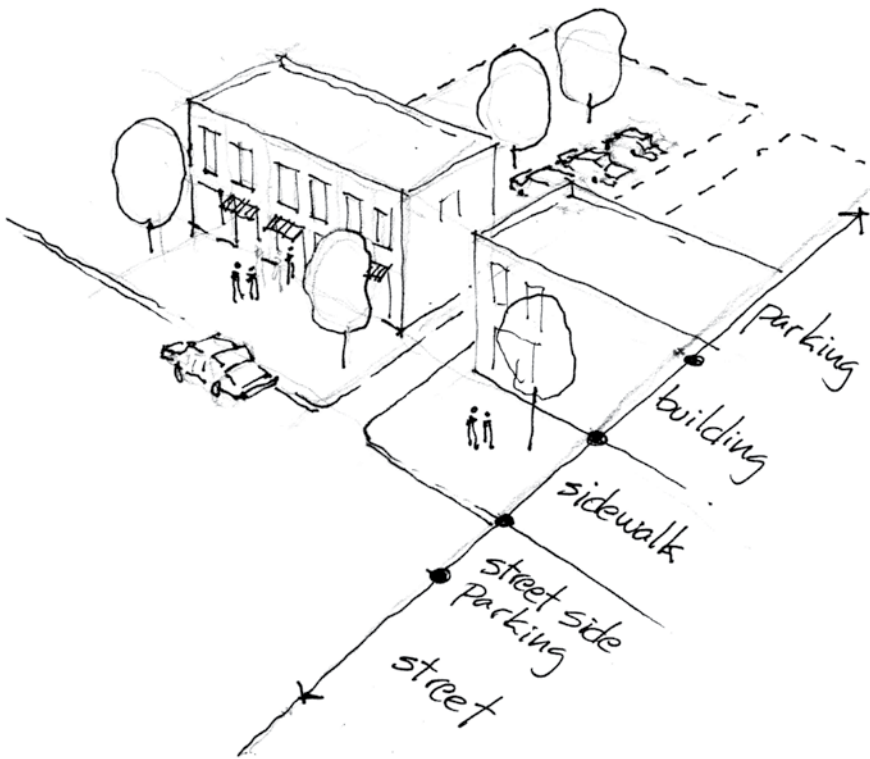
Mitigate the visual impact of parking
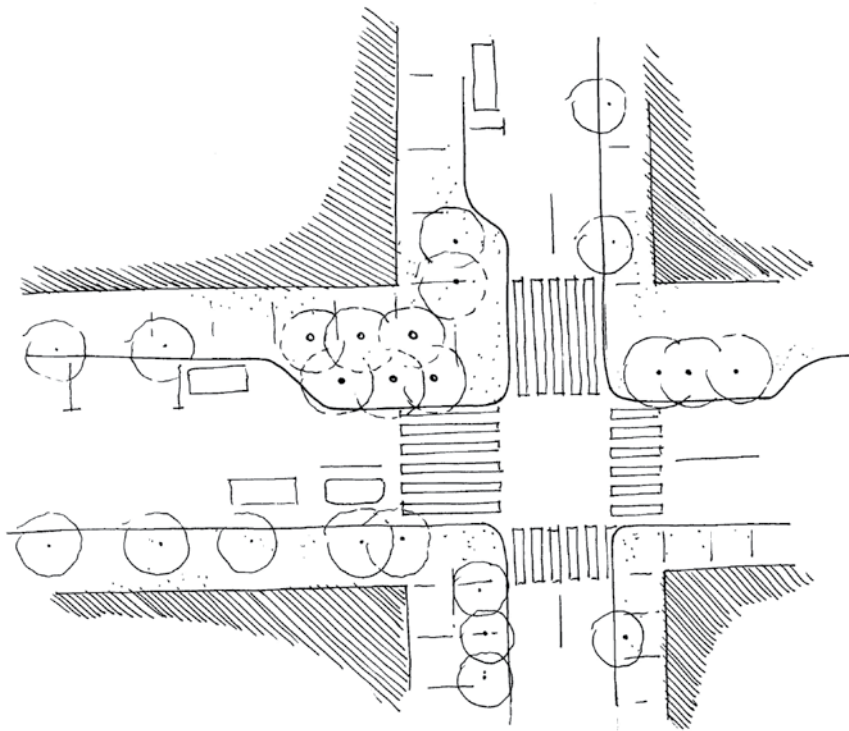
All streets, where possible, should have on street parking. Bulbs can be created at crossings for pedestrian comfort and safety.

7.10 Parking

Goal – To support a diversity of strategies that provide options for parking within Downtown and work towards reducing car dependency.

As drivers approach Downtown, parking is often at the forefront of their mind. Where will I park? How far will it be? These are pressing questions. The priority however, is still to make Downtown a desirable destination. Successful downtowns throughout North America have shown that: a) the perception is always that the demand for parking is greater than the supply; and b) if the destination is worthwhile, people will find a way to reach it.

The strategy for parking should be about supporting access to Downtown. It is one part of the puzzle that will contribute to a lively Downtown.

And while parking is an important function, it is not often beautiful. Therefore, design solutions must be crafted to mitigate the visual impact of parking. If not carefully placed and designed, surface parking lots, structures and access routes to parking areas can detract from the visual quality and character of Downtown and can negatively affect the safety of pedestrians.

On street parking

All streets, where possible, should have on street parking. The design can vary according to demand and the interaction with the sidewalk.

In cases where rush hour traffic will not necessitate the use of the parking lane as a traffic lane, bulb-outs can be created at pedestrian crossings to reduce the number of lanes pedestrians walk across. Main Street has some good examples of this condition.

On street parking should be parallel to the sidewalk (not perpendicular).

Surface Parking

Locate surface parking lots at the interior of the block not at the street edge or at corner locations.

Parking lots under buildings should not extend to the street front. Rather, they should be shielded from the street by the front of the building. In this way the architectural continuity of the street can be preserved. Parking behind a building accessed from an alley is preferred in order to minimize pedestrian conflicts.

Subdivide surface parking lots into smaller areas through the use of landscaping or other visual elements. Planting islands for flowers, ground cover, or shrubs should be used at entrances, exits, internal turns, and to separate double rows of cars. Such islands should be designed to break up the expanse of pavement and help establish the desired direction of circulation, and should be designed to incorporate a strategy for snow removal/storage in winter months. Trees are the most essential form of greenery since they screen cars, provide shade, and frame views.

For areas that require overflow or peak season parking such as large retail areas, consider the use of impervious pavement systems. This strategy is strongly recommended adjacent to environmentally sensitive areas such as the riverfront and in floodplains.

Structured Parking

Design parking structures so that they create a visually attractive and active pedestrian environment through the use of a residential/retail/commercial wrap. All above grade parking structures, in which parking is the principle use, should be wrapped with a two story active use to shield the facility from the street and to make the entire building visually pleasing. The garage wrap should be compatible with surrounding buildings.

Pedestrian routes in structures and lots should be easily identifiable and accessed. Clear visual connections between

a garage, or surface parking lot, and adjacent sidewalks and buildings are desirable. Interior and exterior lighting should be designed for safety as well as night-time appearance.

New structures should merit the same level of materials and finishes as that a principal building would. Care in the use of basic design elements can make a significant improvement.

Residential Garages

Private Garages should be setback from the main frontage and ideally located in the back of the lot, accessible through back lanes or side lanes. Garages that front directly onto the street should be avoided.

Connect driveways for adjacent properties to improve circulation and eliminate the need for more curb cuts.

Service and Loading

In general, parking and loading should not be located where they are perceived from a main street and should be hidden from view. Where possible, access to parking and loading should be located on the road with the least pedestrian traffic. Access to parking and loading areas should never be located at gateway sites, or at the terminus of a view corridor.

Bicycle Parking

Bicycle racks should be conveniently located near building entrances and transit stops so they are highly visible and convenient. To facilitate access, install a curb ramp in any drive near the bike parking.
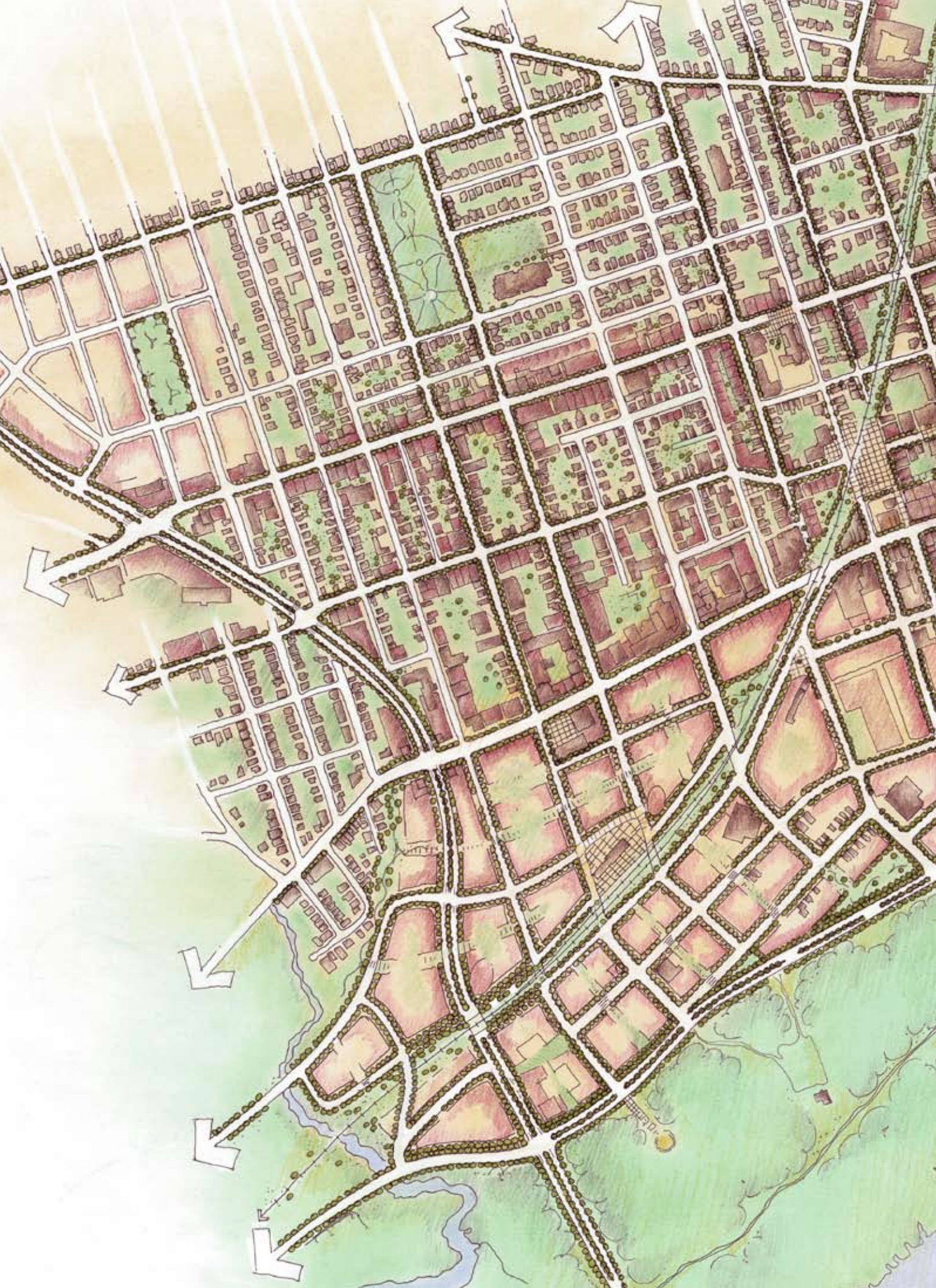
Ce document est également disponible en français.

For further information about the Development Vision, or to obtain a copy of the Background Report and/or Implementation Strategy, visit the website:

http://www.moncton.ca/search/english/cityhall/downtown/downtown.htm

or contact

Corporate Planning and Policy Development
Planification corporative et élaboration des politiques
City of / Ville de MONCTON
506-853-3459

'resurgo'
(I rise again)

In 1875 Moncton regained its charter and
incorporated anew with this motto.